

Plotted By: Jacobson, Matthew Sheet Set: MOVE Culver City Layout: TITLE SHEET Date: June 23, 2025 06:50:19pm Path: K:_LDT_TPT0\ITS\099417014 - MOVE Culver City 2023 Design\CADD\PlanSheets\099417014--TITLE--PHASE2.dwg

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UTILITY COMPANY CONTACTS

COMPANY	CONTACT	PHONE NO.
Underground Utility	Jason Sum	(800) 227-2600
Southern California Gas Company--Distribution	Jennifer Deschepper	(310) 687-2011
Golden State Water Company	Mudila Kangulungu	(562) 907-9200
Southern California Edison	Danny Peterson	(310) 315-3220
Frontier Communications	Joseph Forkert	(310) 264-5125
AT&T	David Bergeron	(714) 963-7964
Shell Pipeline Company LP	Dave Zerler	(310) 816-2063
Chevron Pipeline Company	Ken Chung	(310) 669-4014
MWD--Pipeline Maintenance Group	Mark Dennings	(213) 217-7670
MPOWER Communications		(949) 864-0296

ABBREVIATIONS

STA	STATION
EX	EXISTING
PROP	PROPOSED
PL	PROPERTY LINE
R/W	RIGHT-OF-WAY
MH	MANHOLE
CB	CATCH BASIN
AC	ASPHALT/CONCRETE
CONC	CONCRETE
DWY	DRIVEWAY
FS	FINISH SURFACE
FL	FLOW LINE
TC	TOP OF CURB
BCR	BEGIN OF CURVE
ECR	END OF CURVE
FG	FINISH GRADE
EQ	EQUAL
INV	INVERT
CL	CENTER LINE
W	WIDTH
R.Cb	RED CURB
L	LENGTH
ST	STREET
BLVD	BOULEVARD
O.C.	ON CENTER
S/O	SOUTH OF
N/O	NORTH OF
U.N.O	UNLESS NOTED OTHERWISE

ENGINEER'S NOTICE TO CONTRACTOR

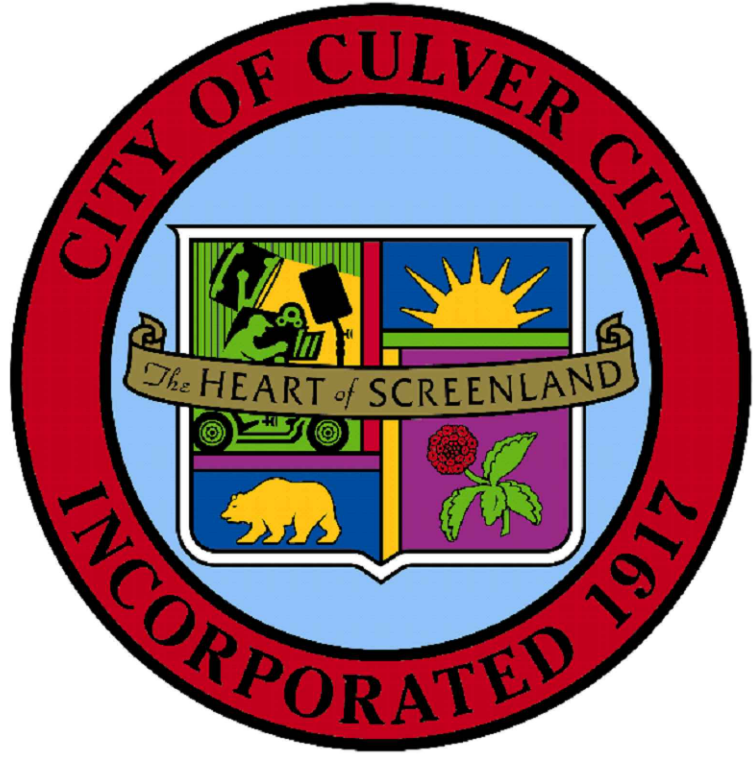
THE EXISTENCE AND LOCATION OF ANY UNDERGROUND UTILITY PIPES OR STRUCTURES SHOWN ON THESE PLANS ARE OBTAINED BY SEARCH OF THE AVAILABLE RECORDS. TO THE BEST OF OUR KNOWLEDGE THERE ARE NO EXISTING UTILITIES EXCEPT AS SHOWN ON THESE PLANS. THE CONTRACTOR IS REQUIRED TO TAKE DUE PRECAUTIONARY MEASURES TO PROTECT THE UTILITY LINES SHOWN AND ANY OTHER LINES NOT OF RECORD OR NOT SHOWN ON THESE PLANS.

THE ESTIMATED QUANTITY FOR EACH SPECIFIC ITEM OF THE WORK DESIGNATED ON THE PLANS SHALL BE CONSIDERED AS APPROXIMATE ONLY AND NO GUARANTEE IS MADE THAT THE QUANTITIES WHICH CAN BE DETERMINED BY COMPUTATIONS, BASED ON THE DETAILS AND DIMENSIONS SHOWN ON THE PLANS, WILL EQUAL THE ESTIMATED QUANTITIES. THE ESTIMATE OF QUANTITIES IS PROVIDED BY THE ENGINEER ONLY FOR THE CONVENIENCE OF THE OWNER, THE CONTRACTOR SHALL MAKE HIS OWN DETERMINATION AND BE RESPONSIBLE FOR HIS OWN CONSTRUCTION QUANTITIES BEFORE SUBMITTING A BID. ANY ITEM OF WORK, OR PORTION THEREOF, REQUIRED BY THESE PLANS WHICH IS NOT SPECIFICALLY LISTED IN THE ESTIMATE OF QUANTITIES SHALL BE CONSIDERED AS INCLUDED IN THE OTHER ITEMS OF WORK.

TAPER LENGTH CALCULATIONS

SHIFT TAPER #1:
L = W X S² / 60 = 4 X 35² / 60 = 82' USE 85'
L = TAPER LENGTH
W = OFFSET
S = DESIGN SPEED

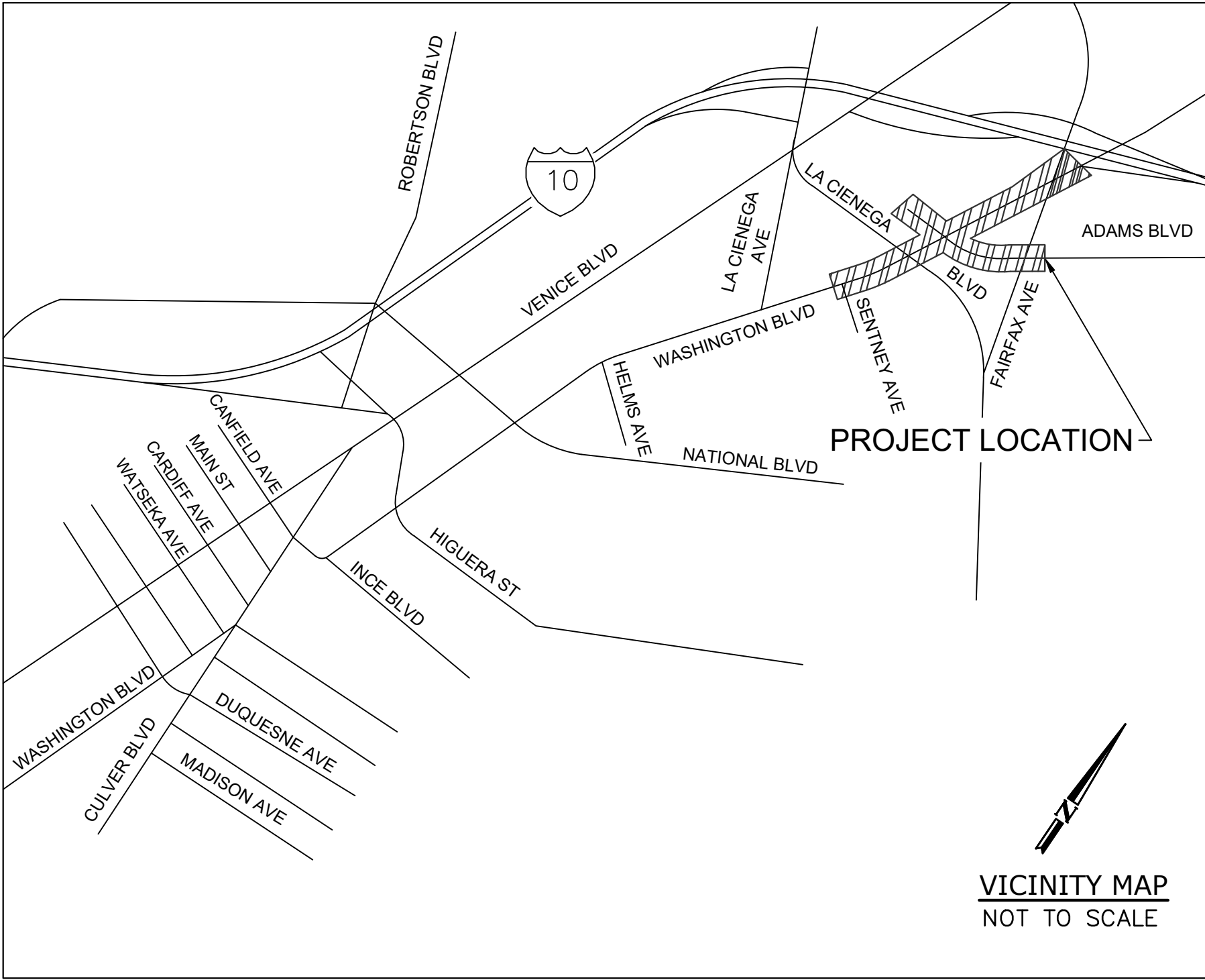
SHIFT TAPER #2:
L = W X S² / 60 = 10 X 35² / 60 = 204' USE 205'
L = TAPER LENGTH
W = OFFSET
S = DESIGN SPEED



MOVE CULVER CITY 2023
PHASE 2
SIGNING & STRIPING AND
TRAFFIC SIGNAL MODIFICATIONS

SHEET INDEX

SHEET NO.	DESCRIPTION
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9	SIGNING AND STRIPING PLAN 5
10	SIGNING AND STRIPING PLAN 6
11	TRAFFIC SIGNAL MODIFICATION PLAN



SYMBOLIC LEGEND

- = RIGHT-OF-WAY
- >--- = EXISTING STRIPING & MARKINGS TO REMAIN.
- x--- = EXISTING STRIPING & MARKINGS TO BE REMOVED.
- x--- = EXISTING PAVEMENT MARKING TO BE REMOVED.
- >--- = PROPOSED STRIPING & MARKINGS.
- ⊥ = EXISTING SIGN AND POST.
- ⊥ = PROPOSED SIGN AND POST.
- ⊥ = PROPOSED ANGLED SIGN AND POST USING 30 DEGREE BRACKET.
- Ⓢ = SIGNALIZED INTERSECTION.
- = EXISTING CURB MARKINGS TO REMAIN.
- x--- = EXISTING CURB MARKINGS TO BE REMOVED.
- = PROPOSED CURB MARKINGS.
- ⦶ = EXISTING DETECTION LOOP
- ⦶ = EXISTING BICYCLE DETECTION LOOP
- DETAIL X | DETAIL Y PER CALTRANS STANDARD PLANS A20A--A20F
- < / > = BEGINNING/END OF CURB RETURN

NO	DESCRIPTION	BY	AP	DATE

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PREPARED BY

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660 S. FIGUEROA STREET, SUITE 2050, LOS ANGELES, CA 90017
PHONE: 213-261-4040
WWW.KIMLEY-HORN.COM

PUBLIC WORKS DEPARTMENT APPROVAL
YANNI DEMITRI, PE, TE DATE
PUBLIC WORKS DIRECTOR / CITY ENGINEER

CITY OF CULVER CITY PUBLIC WORKS DEPARTMENT MOBILITY & TRAFFIC ENGINEERING DIVISION				
MOVE CULVER CITY 2023 PHASE 2 TITLE SHEET				
DESIGNED BY TC,JM,MJ,VC	DATE 06/23/2025	CHECKED BY THOMAS CHECK, PE, TE SENIOR TRAFFIC ENGINEER	DATE	SHEET 1
DRAWN BY M.J.VC,RU	DATE 06/23/2025	APPROVED BY ANDREW MAXIMOUS, PE, TE MOBILITY & TRAFFIC ENGINEERING MANAGER	DATE	OF 11 SHEETS
PROJECT NUMBER			PS017	PLAN MTE1002

SIGNING AND STRIPING GENERAL NOTES:

1. ALL MATERIAL AND WORK SHALL CONFORM TO THE LATEST EDITIONS THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CALIFORNIA MUTCD), STANDARD PLANS AND STANDARD SPECIFICATIONS OF THE CALIFORNIA DEPARTMENT OF TRANSPORTATION, FHWA STANDARD HIGHWAY SIGNS, CALIFORNIA SIGN SPECIFICATION, AND THE SPECIAL PROVISIONS.
2. ALL STRIPING AND PAVEMENT LEGENDS SHALL BE ALKYD-BASED THERMOPLASTIC WITH RAISED PAVEMENT MARKERS UNLESS OTHERWISE NOTED.
3. ALL CONFLICTING LINES AND LEGENDS SHALL BE REMOVED BY WET BLASTING OR GRINDING. REMOVAL OF RAISED PAVEMENT MARKERS SHALL NOT CAUSE STRUCTURAL DAMAGE TO THE PAVEMENT.
4. ALL SALVAGED SIGNS AND POSTS SHALL BE DELIVERED TO THE CITY MAINTENANCE YARD, 9505 JEFFERSON BLVD, CITY OF CULVER CITY, (310) 253-6441, UNLESS OTHERWISE INDICATED.
5. NEW SIGN POSTS SHALL BE "UNISTRUT" SQUARE GALVANIZED STEEL POSTS WITH A BREAKAWAY FEATURE, OR APPROVED EQUAL, PER LADOT STANDARD PLAN S-452.0.
6. ALL NEW STREET NAME SIGNS SHALL BE DOUBLE-SIDED RETRO-REFLECTORIZED TYPE WITH A GREEN BACKGROUND AND WHITE LEGEND. POST MOUNTED STREET NAME SIGNS SHALL USE 6 INCH UPPER CASE AND 4 ½ INCH LOWER CASE SERIES C LETTERS WITH THE CITY SEAL AND LOGO, AS PER CITY STANDARD PLAN. ALL OVERHEAD STREET NAME SIGNS SHALL USE 12 INCH UPPER CASE AND 9 INCH LOWER CASE SERIES C LETTERS AND SHALL BE INTERNALLY-ILLUMINATED USING LEDS AS INDICATED ON THE PLANS. SEE THE SPECIFICATIONS UNDER TRAFFIC SIGNALS.
7. ALL NEW SIGNS ARE TO HAVE RETRO-REFLECTORIZED SHEETING WITH TYPE III BACKGROUNDS, TYPE VIII WHITE TEXT, AND UV/ANTI-GRAFFITI FILM.
8. ALL SIGNS ARE TO BE PER THE STATE OF CALIFORNIA SIGN SPECIFICATIONS AND SHALL BE MINIMUM 0.1 INCH THICK ALUMINUM SHEETING.
9. All INSTALLATIONS AND REMOVALS OF SIGNS AND COLORED CURB ZONES ARE TO BE IMPLEMENTED BEFORE STRIPING MARKOUT.
10. STRIPING SHALL BE MARKED OUT BY THE CONTRACTOR BY USE OF A THIN PAINTED LINE OVER A PULLED ROPE OR STRING. YELLOW MARKOUT IS TO BE USED FOR CENTERLINES AND YELLOW CROSSWALKS AND WHITE MARKOUT IS TO BE USED FOR ALL OTHER LINES. THE MARKOUT FOR DOUBLE YELLOW LINES AND BARRIER LINES IS TO BE A SOLID THIN LINE. AT INTERSECTIONS WITH CURVES OR WITH WIDTH CHANGES THE MARKOUT SHOULD BE PULLED ACROSS THE INTERSECTION TO ENSURE NO OFFSETS IN THE STRIPING. THE MARKOUT IS TO BE APPROVED BY THE CITY TRAFFIC ENGINEER FOR ALIGNMENT PRIOR TO FINAL INSTALLATION. A MINIMUM OF ONE WEEK NOTICE IS REQUIRED BY THE CITY TRAFFIC ENGINEER FOR APPROVAL OF STRIPING.
11. TABS (TEMPORARY ADHESIVE DELINEATORS) SHALL BE INSTALLED IMMEDIATELY FOLLOWING STRIPING REMOVAL ALONG THE GHOST LINES OF THE REMOVED STRIPING. TABS ALSO SHALL BE INSTALLED IMMEDIATELY FOLLOWING RESURFACING BY ESTIMATING THE POSITIONING OF THE NEW STRIPING. IMMEDIATELY FOLLOWING THE INSTALLATION OF THE MARKOUT THE OLD TABS ARE TO BE REMOVED AND NEW TABS ARE TO BE INSTALLED ADJACENT TO THE MARKOUT, UNLESS THE OLD TABS ARE WITHIN ONE FOOT OF THE MARKOUT. ALL TABS SHOULD BE REMOVED AFTER THE FINAL STRIPING IS INSTALLED. TABS SHALL BE THE SAME COLOR AS THE FINAL STRIPING AND DUAL TABS SHALL BE USED FOR LINES WIDER THAN FOUR INCHES.

TRAFFIC SIGNAL GENERAL NOTES:

1. ALL MATERIAL AND WORK SHALL CONFORM TO THE LATEST EDITIONS OF THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CALIFORNIA MUTCD), STANDARD PLANS AND STANDARD SPECIFICATIONS OF THE CALIFORNIA DEPARTMENT OF TRANSPORTATION, FHWA STANDARD HIGHWAY SIGNS, CALIFORNIA SIGN SPECIFICATION, AND THE SPECIAL PROVISIONS. TRAFFIC SIGNAL POLES AND FOUNDATIONS SHALL CONFORM TO THE 2010 EDITION OF THE CALIFORNIA DEPARTMENT OF TRANSPORTATION STANDARD PLANS AND STANDARD SPECIFICATIONS
2. ALL MATERIAL AND EQUIPMENT SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR NEW AND UNUSED, UNLESS OTHERWISE NOTED.
3. SIGNAL POLES, CONTROLLER CABINET, PULL BOXES, AND SERVICE CABINET SHALL BE CONSISTENT WITH THE DESIGN PLANS BUT MAY BE ADJUSTED BY THE CITY SIGNAL TECHNICIAN IN THE FIELD AS NECESSARY.
4. ALL WIRING SHALL BE MARKED (TAGGED) WITHIN THE CONTROLLER CABINET AND PULL BOXES FOR PHASE IDENTIFICATION.
5. UNLESS SHOWN OTHERWISE, DETECTOR LOOPS SHALL BE INSTALLED PER CULVER CITY STANDARDS. THE NEAR EDGE OF LIMIT LINE DETECTORS SHALL BE ONE FOOT UPSTREAM FROM THE EDGE OF THE LIMIT LINE. ADVANCE DETECTORS SHALL BE SEPARATELY WIRED TO EACH LANE AND CENTERED IN EACH LANE.
6. ALL NEW TRAFFIC SIGNAL HEAD AND PEDESTRIAN HEAD HOUSINGS SHALL BE BLACK POLYCARBONATE WITH COLOR RETENTION. NEW TRAFFIC SIGNAL INDICATIONS SHALL BE 12" LED TYPE AND PEDESTRIAN INDICATIONS SHALL BE TWO-SECTIONS WITH LED LAMPS AND WITH COUNTDOWN NUMERALS, UNLESS OTHERWISE SPECIFIED.
7. ALL LOOPS SHALL BE PROPERLY LABELED IN THE PULL BOXES.
8. ALL MULTI-CONDUCTOR CABLES SHALL BE LABELED PER PHASE IN EACH PULL BOX IF THE COLOR INDICATION CHANGES.
9. ALL SIGNAL EQUIPMENT SHALL BE WIRED IN ACCORDANCE WITH THE SIGNAL PHASE DIAGRAM.
10. NEW CONDUIT SHALL BE 4" PVC SCHEDULE 80, UNLESS OTHERWISE NOTED.
11. NEW PULL BOXES SHALL BE No. 6 UNLESS OTHERWISE NOTED.
12. NEW PEDESTRIAN PUSH BUTTONS SHALL BE ACCESSIBLE PEDESTRIAN SIGNALS PER LADOT STD. PLAN S-73.2
13. EMERGENCY VEHICLE PREEMPTION DEVICE SHALL BE OPTICOM MODEL 721. INSTALL M138 DETECTOR CABLE TO OPTICOM DISCRIMINATOR INSIDE CONTROLLER CABINET. OPTICOM CABLE TO BE UNSPLICED FROM CABINET TO DETECTOR.
14. NEW SIGNAL VISORS SHALL HAVE A FULL CIRCLE CROSS SECTION. THEY SHALL BE REMOVABLE BLACK POLYCARBONATE UNITS WITH COLOR RETENTION. STANDARD VISORS SHALL HAVE A LENGTH OF 12". LONG VISORS, WHERE SPECIFIED, SHALL HAVE A LENGTH OF 27". BEVELED VISORS, WHERE SPECIFIED, SHALL HAVE A LENGTH ON THE SHORT SIDE OF 12" AND A LENGTH ON THE LONG SIDE OF 27".
15. NEW LUMINAIRES SHALL BE INDUCTION, CUT-OFF TYPE AND SHALL HAVE BOTH INTEGRAL BALLASTS AND INTEGRAL PHOTO ELECTRIC CONTROLS.
16. MAST-ARM MOUNTED STREET NAME SIGNS SHALL BE PROVIDED USING INTERNALLY-ILLUMINATED LED LIGHT SOURCES AS INDICATED ON THE PLANS. THE SIGN PANELS, SIGN FRAME AND MOUNTING HARDWARE SHALL BE TEMPLE EDGE-LIT RAZOR INTERNALLY-ILLUMINATED LED STREET NAME SIGNS, OR CITY APPROVED EQUIVALENT. THE SIGN PANELS SHALL BE DISPLAYED ON BOTH SIDES WITH GREEN BACKGROUND AND WHITE LETTERING. THE SIGN LAYOUT SHALL BE APPROVED BY THE CITY TRAFFIC ENGINEER.
17. PRIOR TO WORKING ON SERVICE OR LIGHTING CIRCUITS, CONTACT MAINTENANCE OPERATIONS AT (310) 253-6433, 48 HOURS IN ADVANCE, FOR DAILY SAFETY CIRCUIT CLEARANCE.
18. ALL SALVAGED TRAFFIC SIGNAL EQUIPMENT SHALL BE RETURNED TO CITY STORAGE YARD AT 9505 JEFFERSON BLVD. CONTACT FACILITIES MAINTENANCE SUPERVISOR AT (310) 253-6433.

19. STREET LIGHTS SHALL REMAIN OPERATIONAL AT ALL TIMES DURING CONSTRUCTION.
20. WHENEVER NEW CONDUCTORS ARE TO BE INSTALLED IN A CONDUIT WITH EXISTING INDIVIDUAL CONDUCTORS (EXCEPT SERVICE WIRES), ALL INDIVIDUAL CONDUCTORS AT THE INTERSECTION SHALL BE REMOVED AND REPLACED WITH MULTI-CONDUCTOR CABLE. 28-CONDUCTOR CABLE SHALL BE INSTALLED IN ALL NEW STREET CROSSINGS UNLESS OTHERWISE SPECIFIED ON THE PLANS.
21. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN THE LOCATION OF SUCH UNDERGROUND FACILITIES WHICH MAY BE SUBJECT TO DAMAGE BY THE CONTRACTOR'S OPERATIONS. THIS SHALL INCLUDE BUT NOT BE LIMITED TO POT HOLING TO EXPOSE ALL UTILITY CROSSINGS WITH PROPOSED CONDUIT RUNS, TRAFFIC SIGNAL FOUNDATIONS, ETC.
22. NOTE TO TRAFFIC SIGNAL DESIGN CONSULTANT: A SIGNAL TIMING CHART SHALL BE PREPARED FOR NEWLY SIGNALIZED INTERSECTIONS AND MODIFICATIONS TO AN EXISTING TRAFFIC SIGNAL. THE SIGNAL TIMING CHART SHALL BE PREPARED BY THE CONSULTANT WITHIN 30 DAYS OF CITY ACCEPTANCE OF THE ASSOCIATED TRAFFIC SIGNAL PLAN. PRIOR TO SUBMITTAL TO THE CITY OF THE SIGNAL TIMING CHART, THE CONSULTANT SHALL MEET WITH THE DESIGNATED TRAFFIC ENGINEER TO DISCUSS TIMING PARAMETERS. UPON DEVELOPING THE INITIAL TIMING CHART THE CONSULTANT SHALL TEST THE TIMING IN ORDER TO ENSURE THAT IT IS COMPATIBLE WITH PROGRAM SOFTWARE AND THAT IT FUNCTIONS AS DESIRED. THEN, THE CONSULTANT SHALL MEET AND PRESENT TIMING CHART AND SIMULATION RESULTS JOINTLY TO THE DESIGNATED TRAFFIC ENGINEER AND TRAFFIC SIGNAL TECHNICIAN. THE TIMING CHART MAY NOT BE IMPLEMENTED UNTIL APPROVED BY THE DESIGNATED TRAFFIC ENGINEER. WHENEVER THERE IS A CHANGE IN SIGNAL PHASING THE CONSULTANT SHALL BE PRESENT WITH THE SIGNAL TECHNICIAN TO IMPLEMENT THE NEW TIMING IN THE FIELD AND TO TROUBLE SHOOT ANY PROBLEMS THAT MAY ARISE.
23. CONDUCTOR SCHEDULE IS FURNISHED AS AN INSTALLATION GUIDELINE ONLY. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE THE CORRECT CONDUCTORS REQUIRED FOR THE INTENDED OPERATION.
24. ALL NEW PULLBOXES SHALL BE NO. 6 CHRISTY FIBERLITE MODEL NO. FL36TBOX18 WITH FL 36X8 EXTENSION (8-INCH) AND COVER MODEL NO. FL36D WITH "TRAFFIC SIGNAL" INSCRIBED ON THE TOP, UNLESS OTHERWISE NOTED.
25. ALL NEW SIGNAL HEADS SHALL BE OF THE MCCAIN POLYCARBONATE TYPE.
26. ALL NEW TERMINAL COMPARTMENTS SHALL BE BRONZE.

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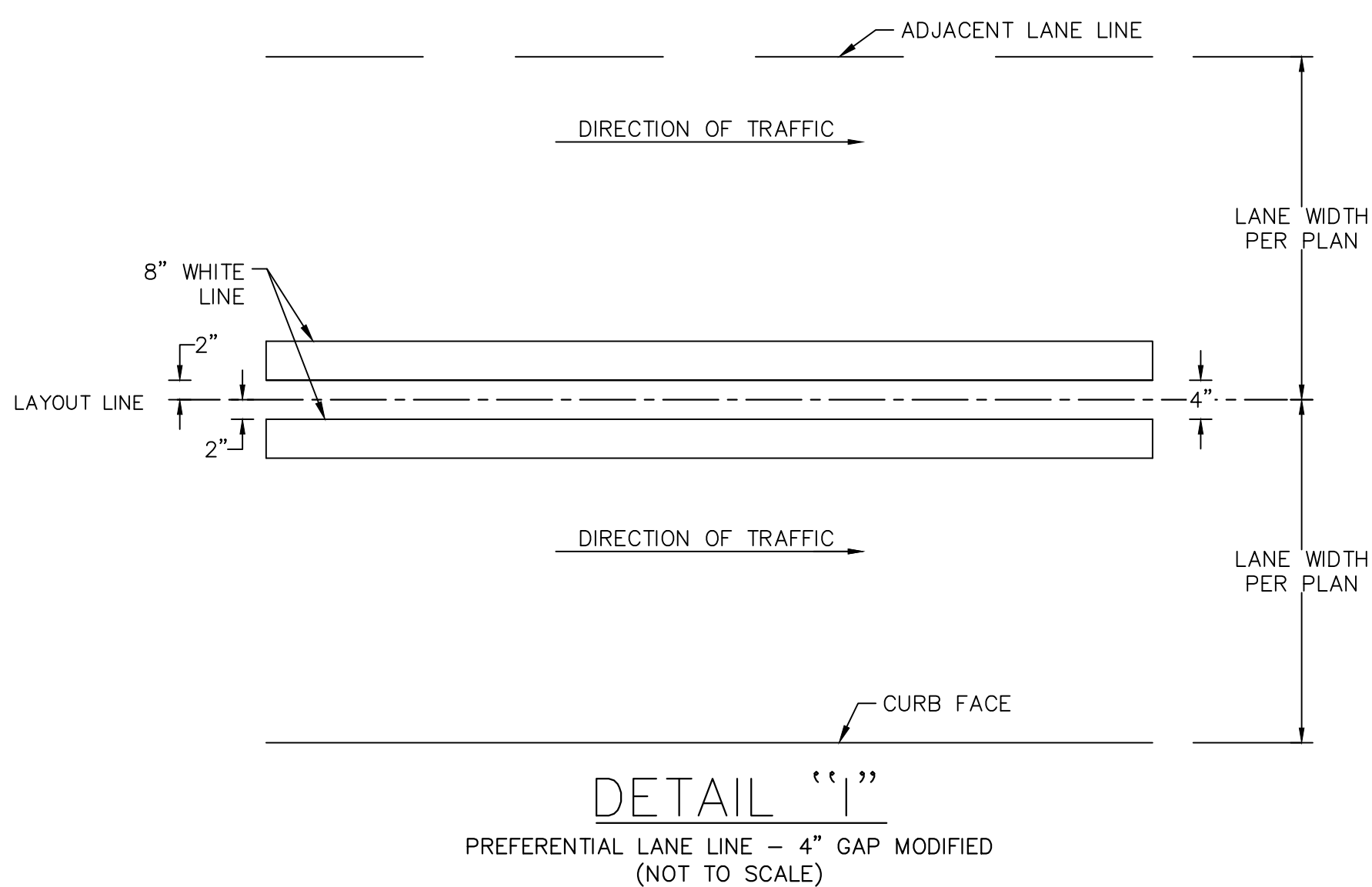
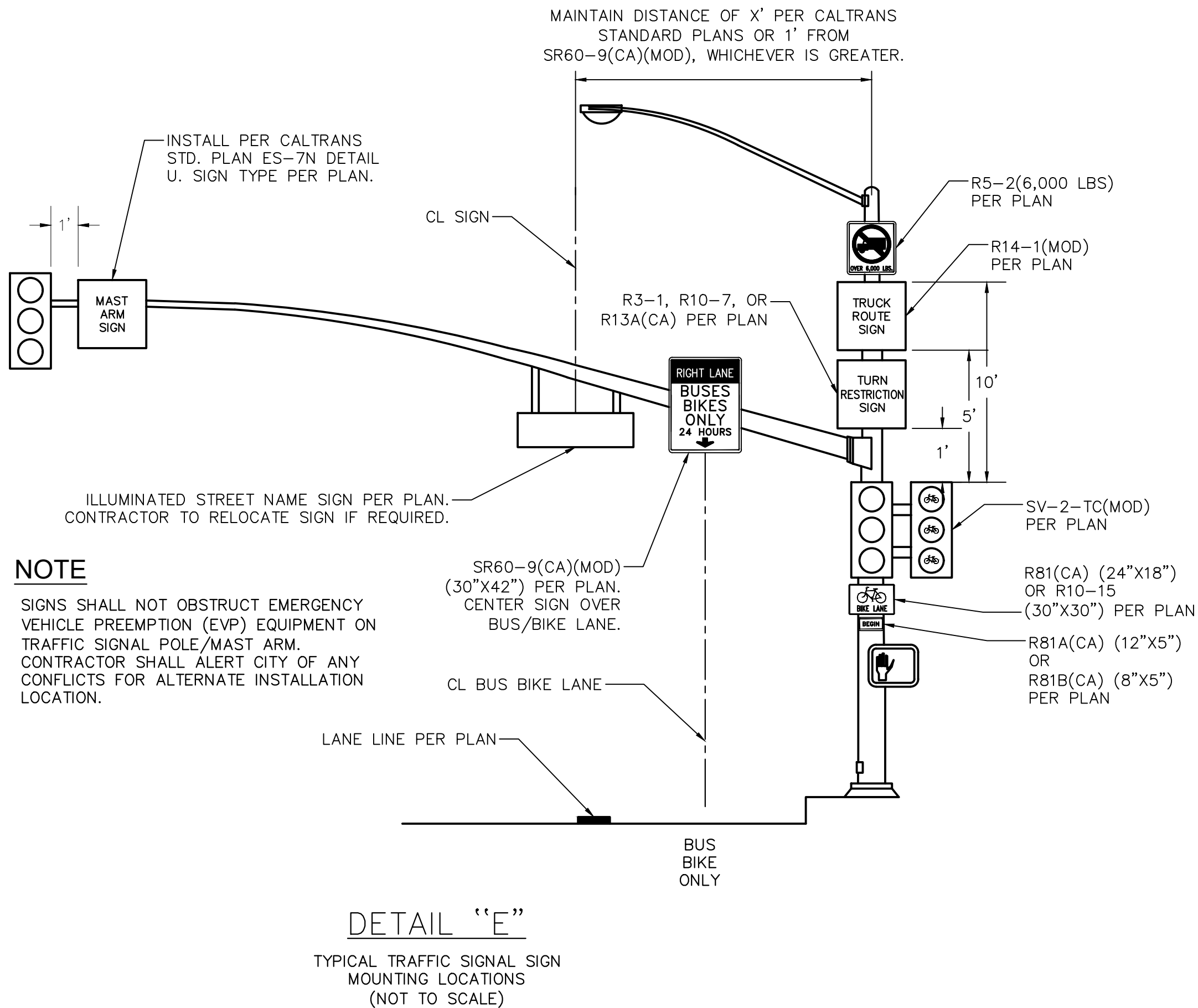
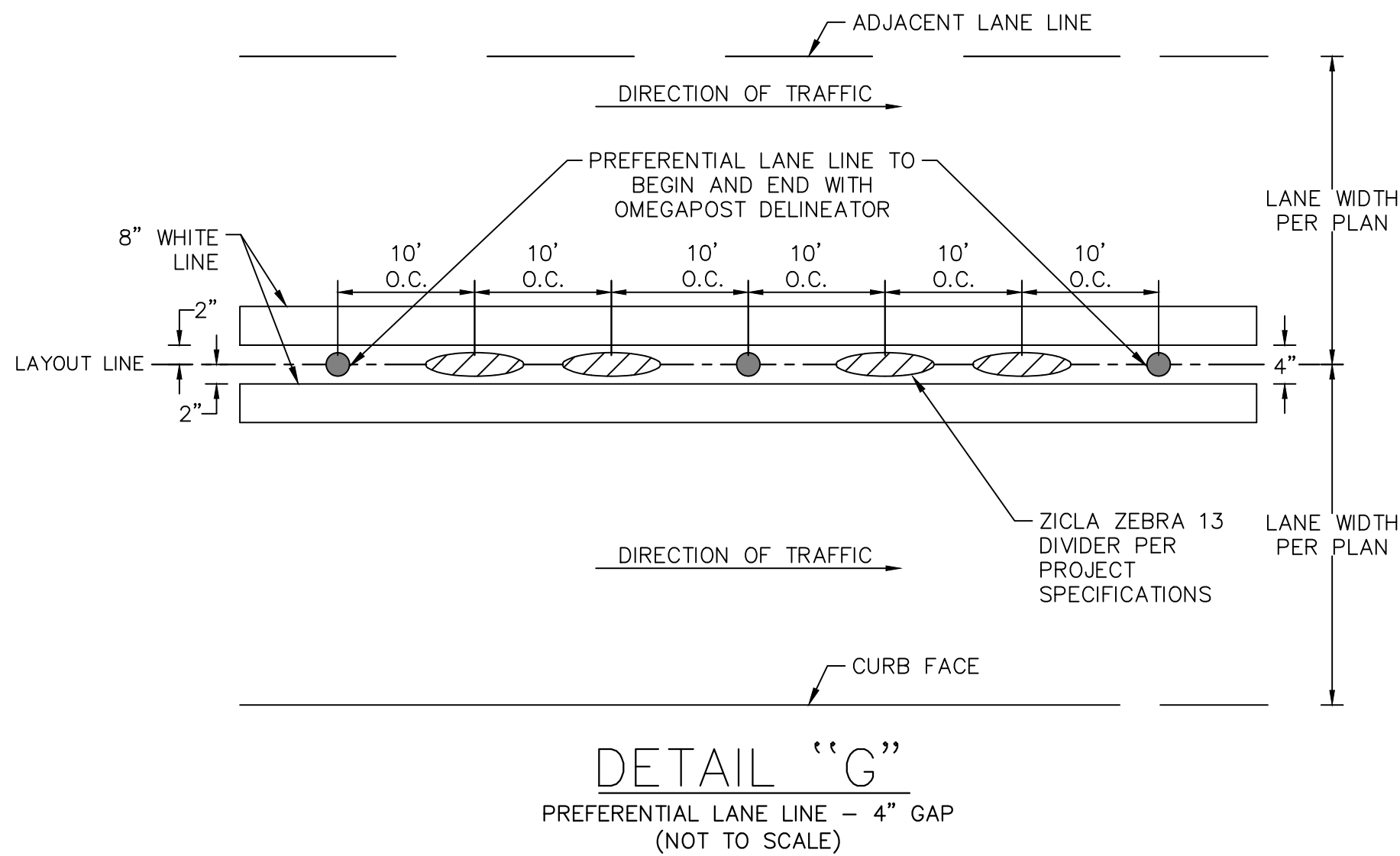
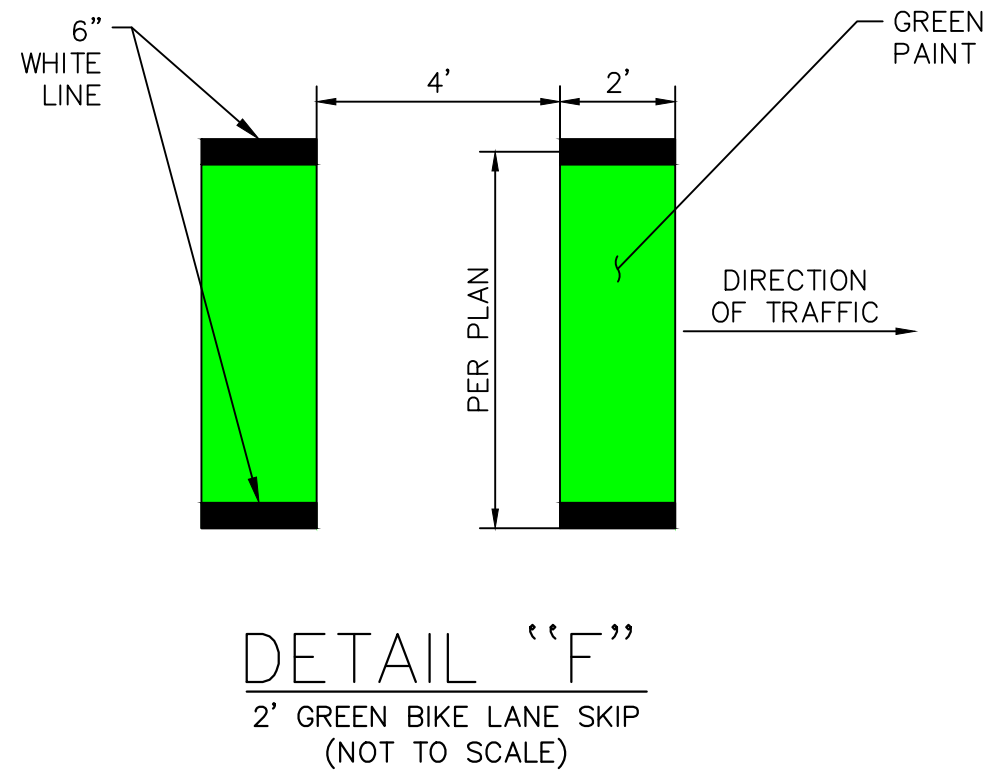
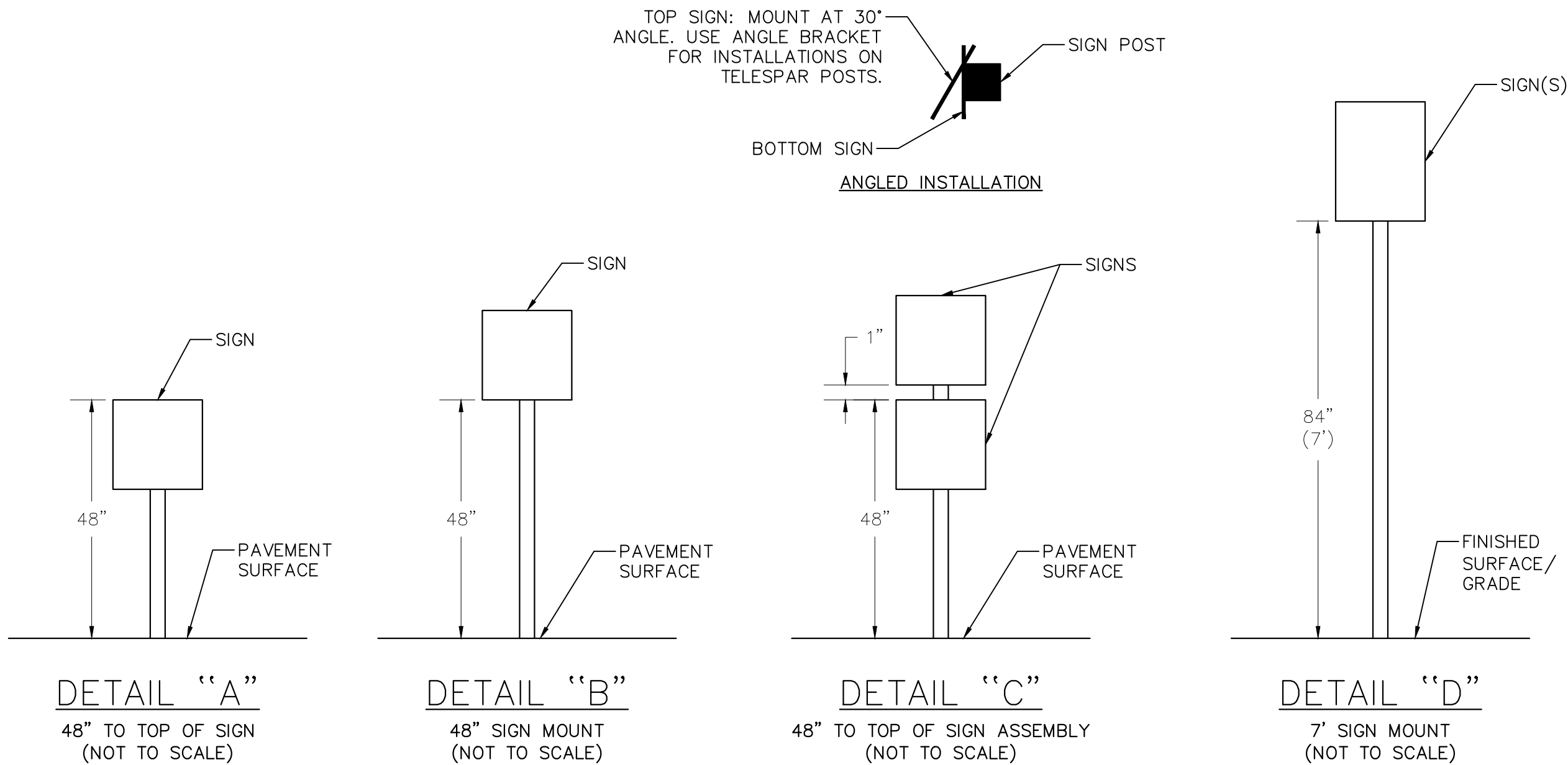
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660 S. FIGUEROA STREET, SUITE 2050, LOS ANGELES, CA 90017
PHONE: 213-261-4040
WWW.KIMLEY-HORN.COM

CITY OF CULVER CITY PUBLIC WORKS DEPARTMENT MOBILITY & TRAFFIC ENGINEERING DIVISION					
MOVE CULVER CITY 2023 PHASE 2 GENERAL NOTES					
DESIGNED BY TC,JM,MJ,VC	DATE 06/23/2025	CHECKED BY THOMAS CHECK, PE, TE SENIOR TRAFFIC ENGINEER	DATE	SHEET 2 OF 11 SHEETS	PROJECT NUMBER PS017 PLAN NUMBER MTE1002
DRAWN BY M.J.VC,RU	DATE 06/23/2025	APPROVED BY ANDREW MAXIMOUS, PE, TE MOBILITY & TRAFFIC ENGINEERING MANAGER	DATE		

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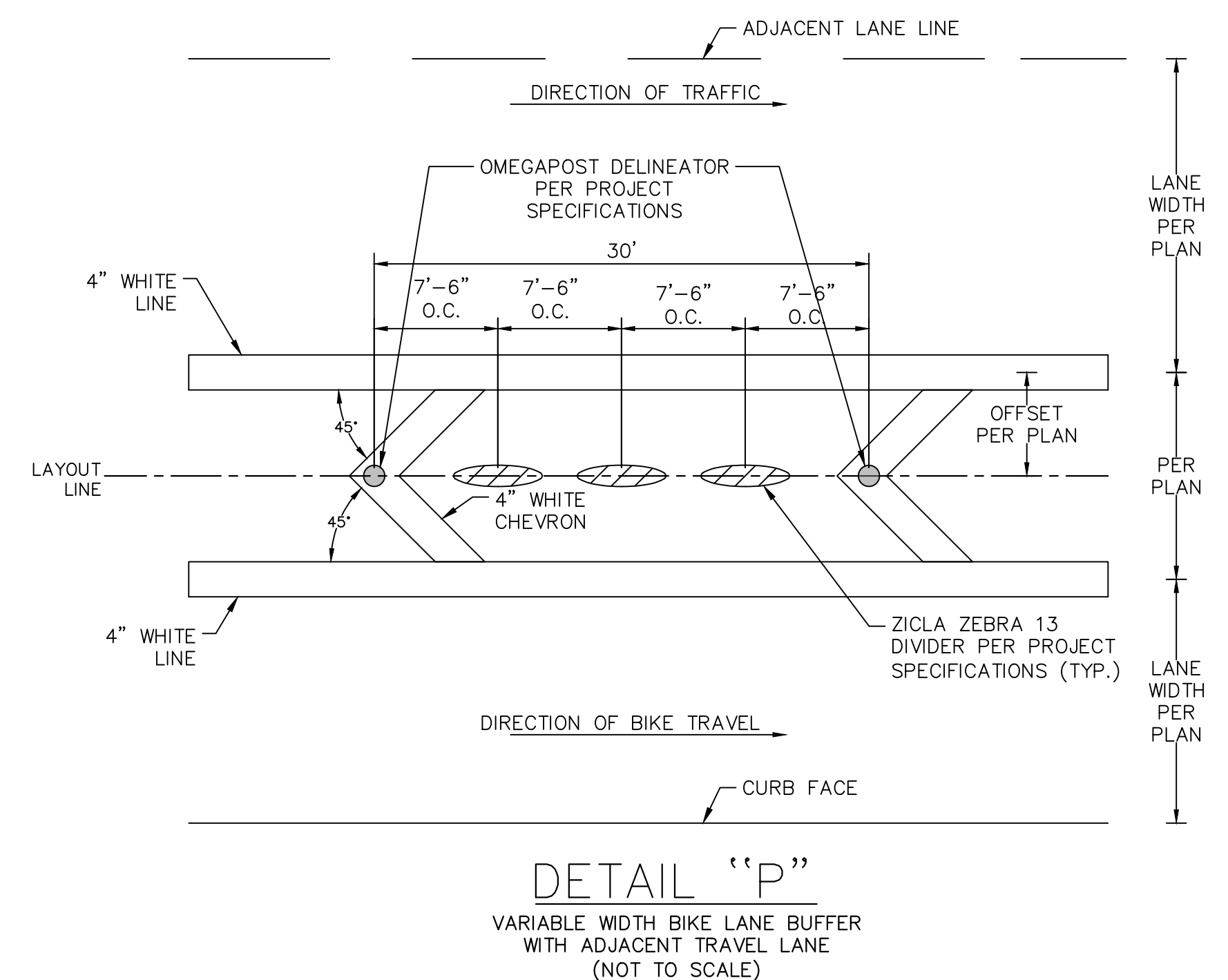
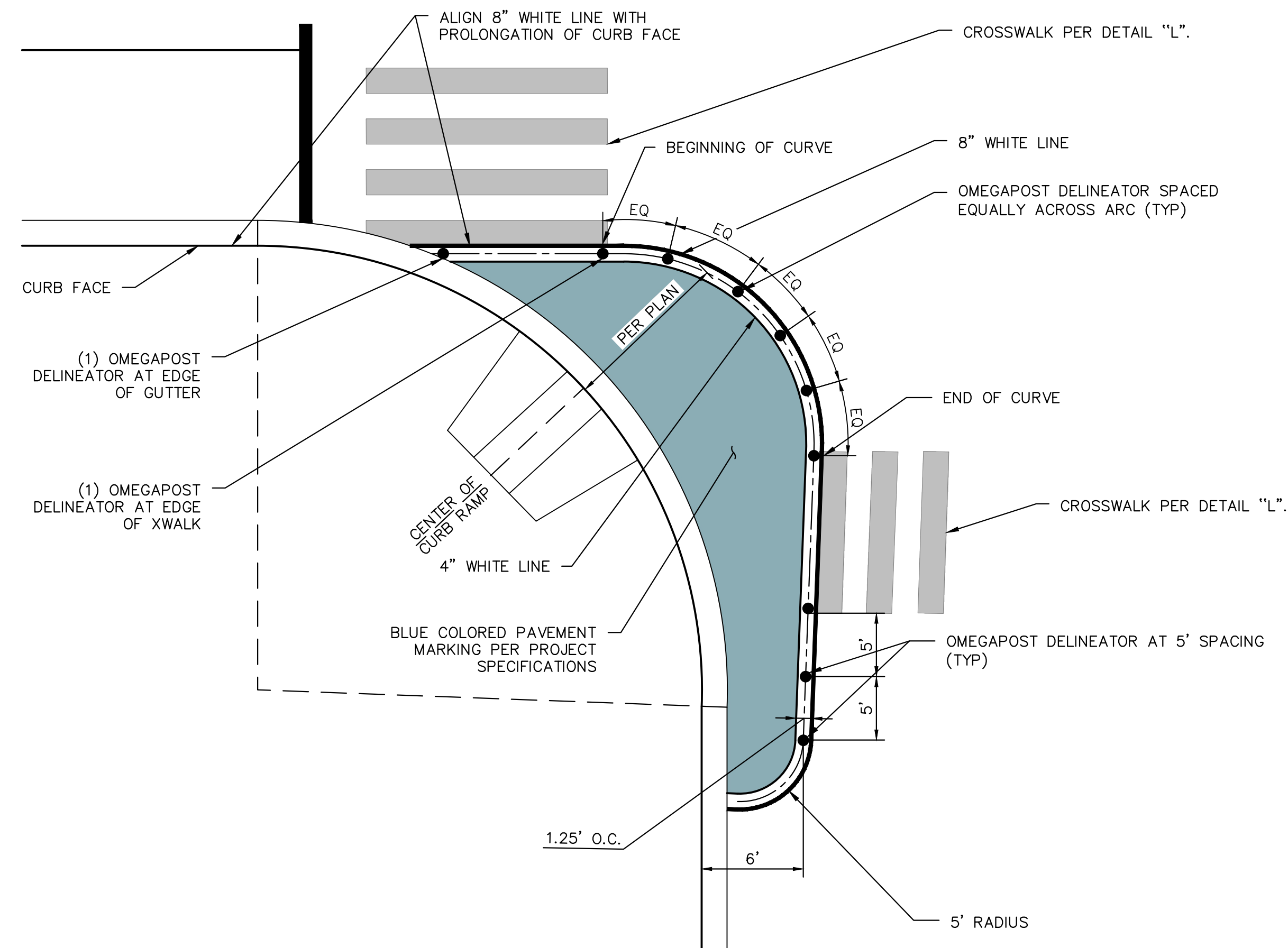
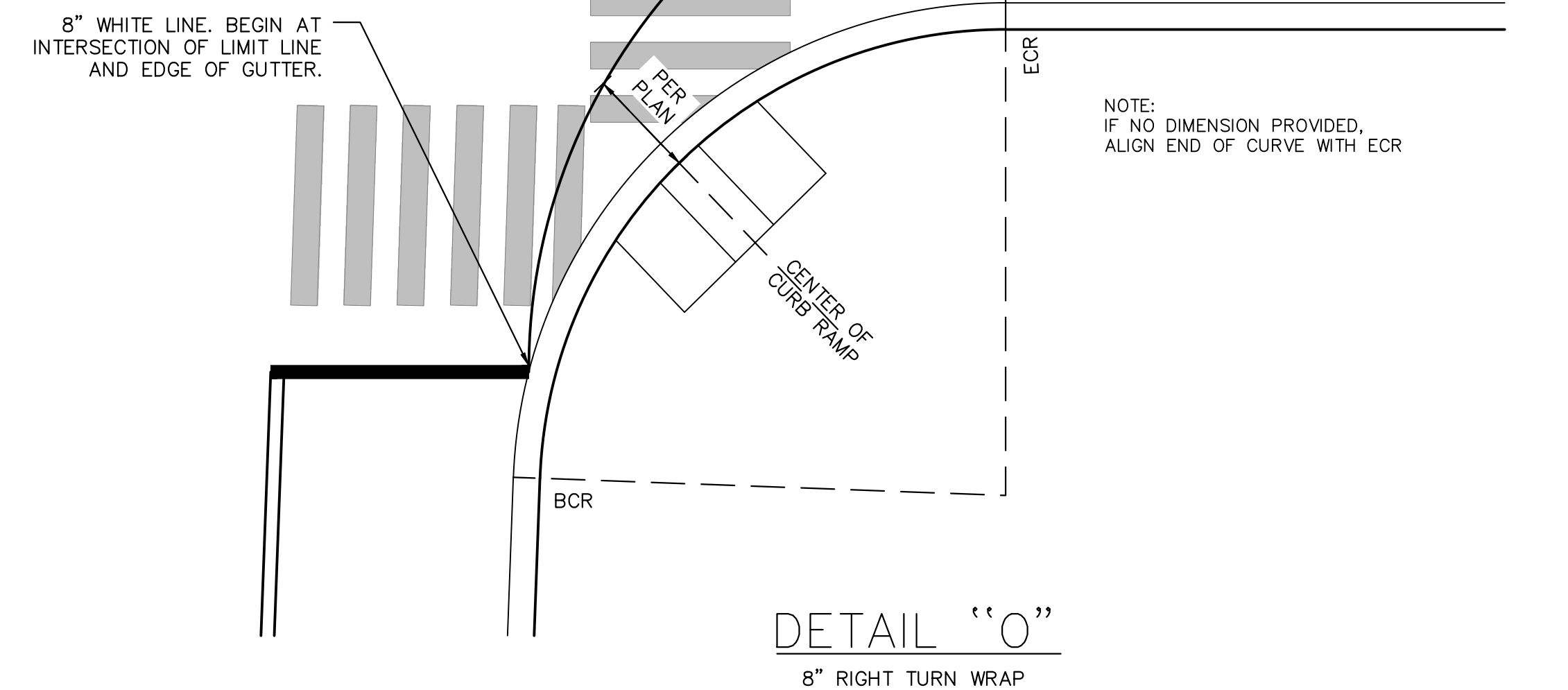
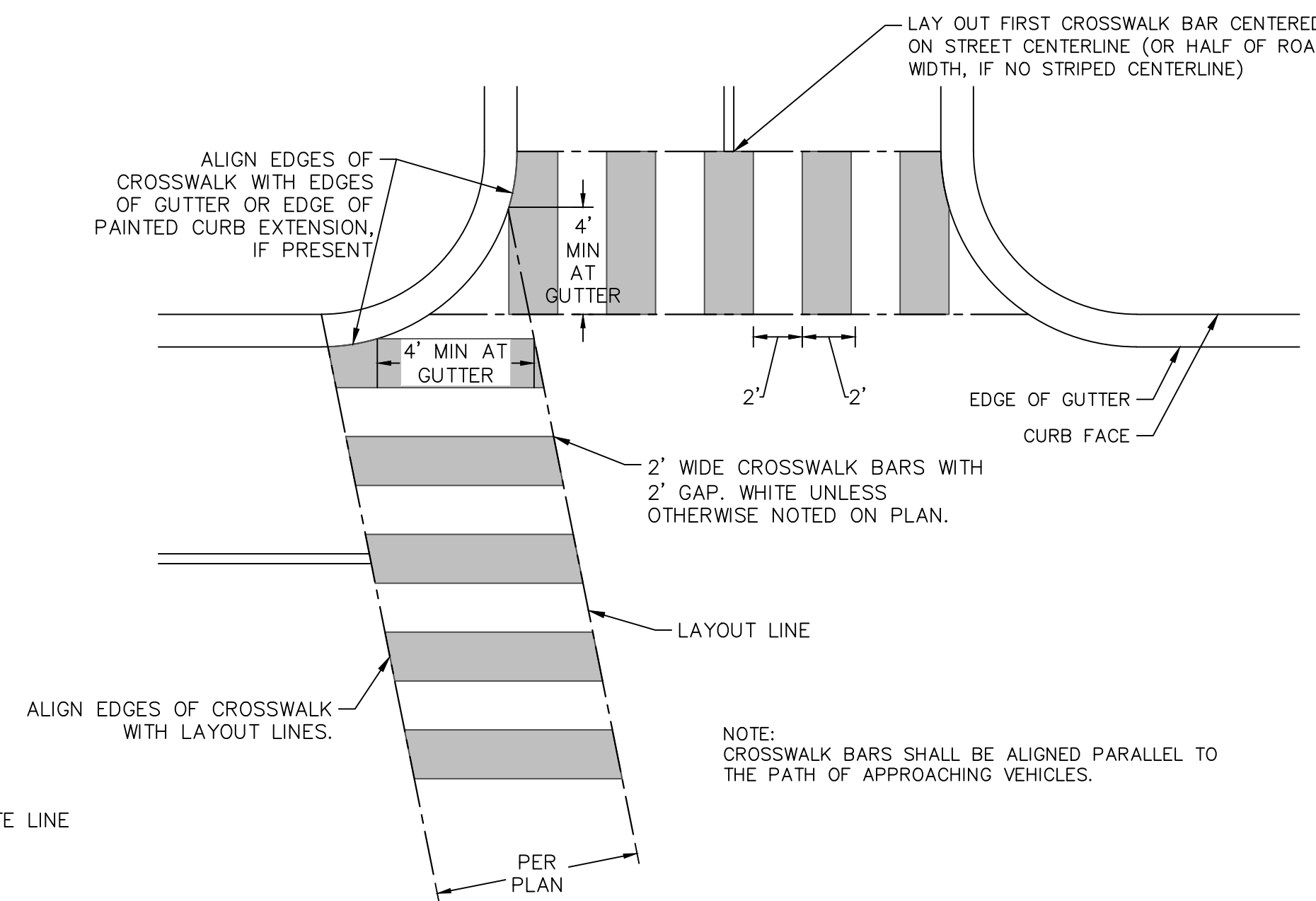
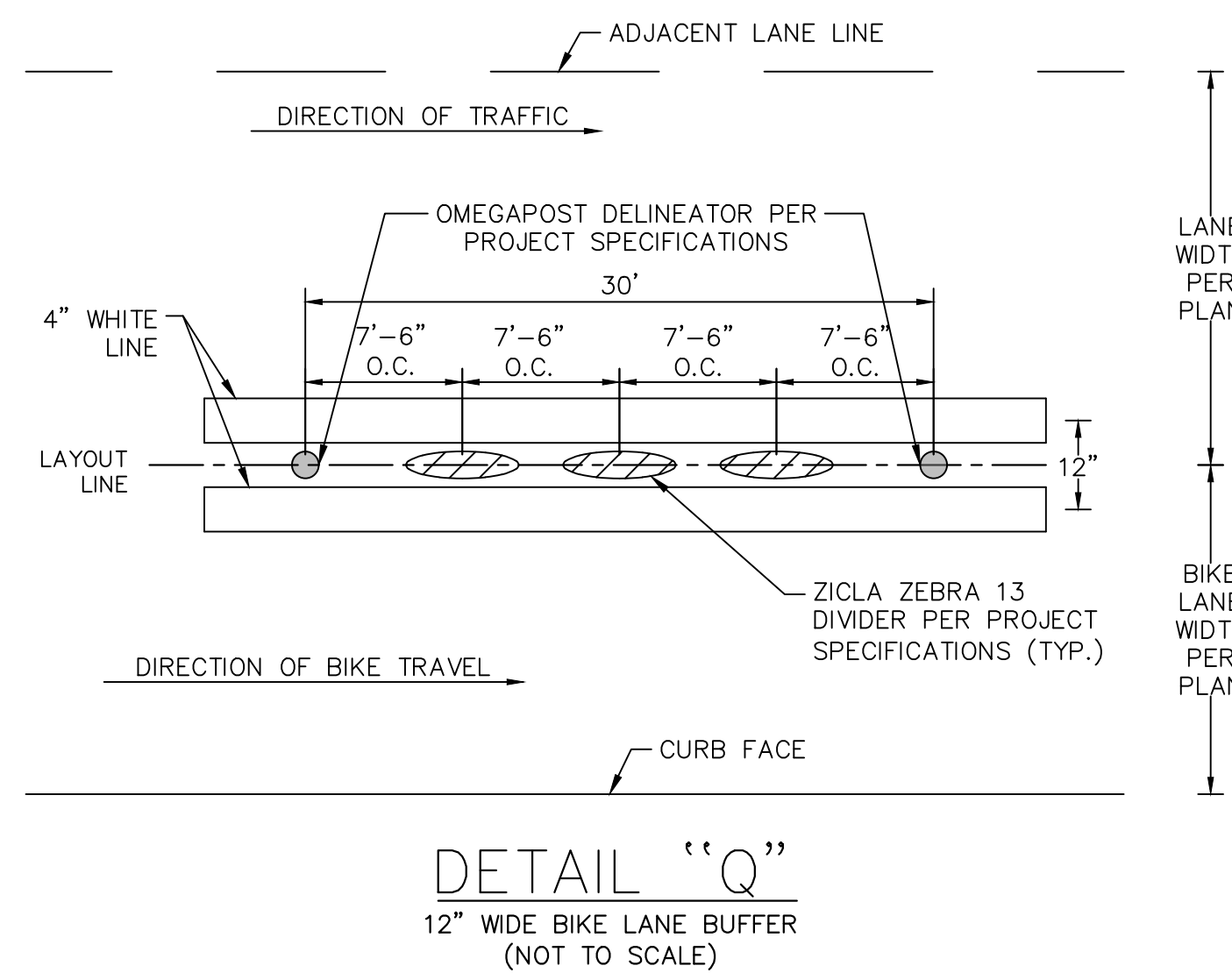
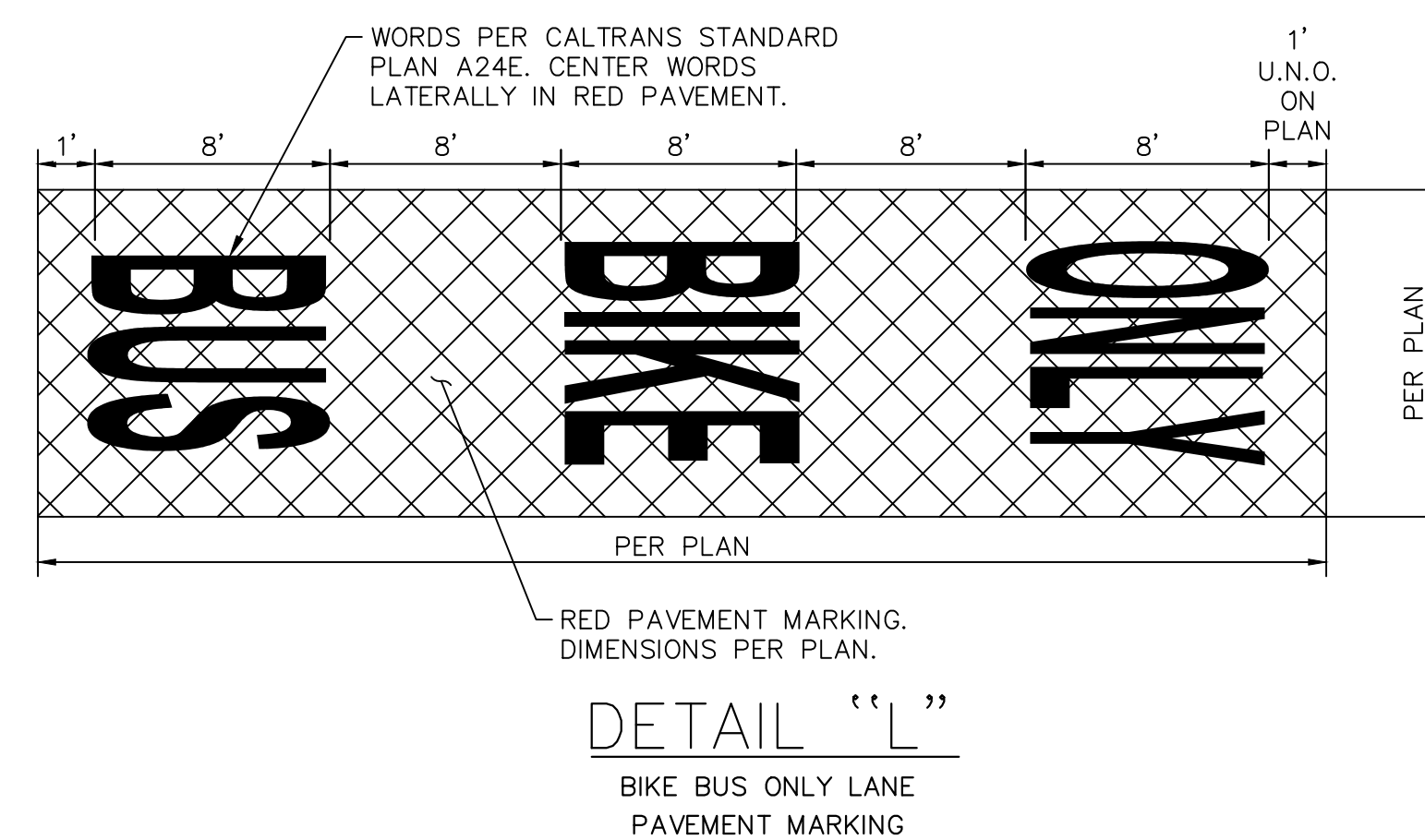
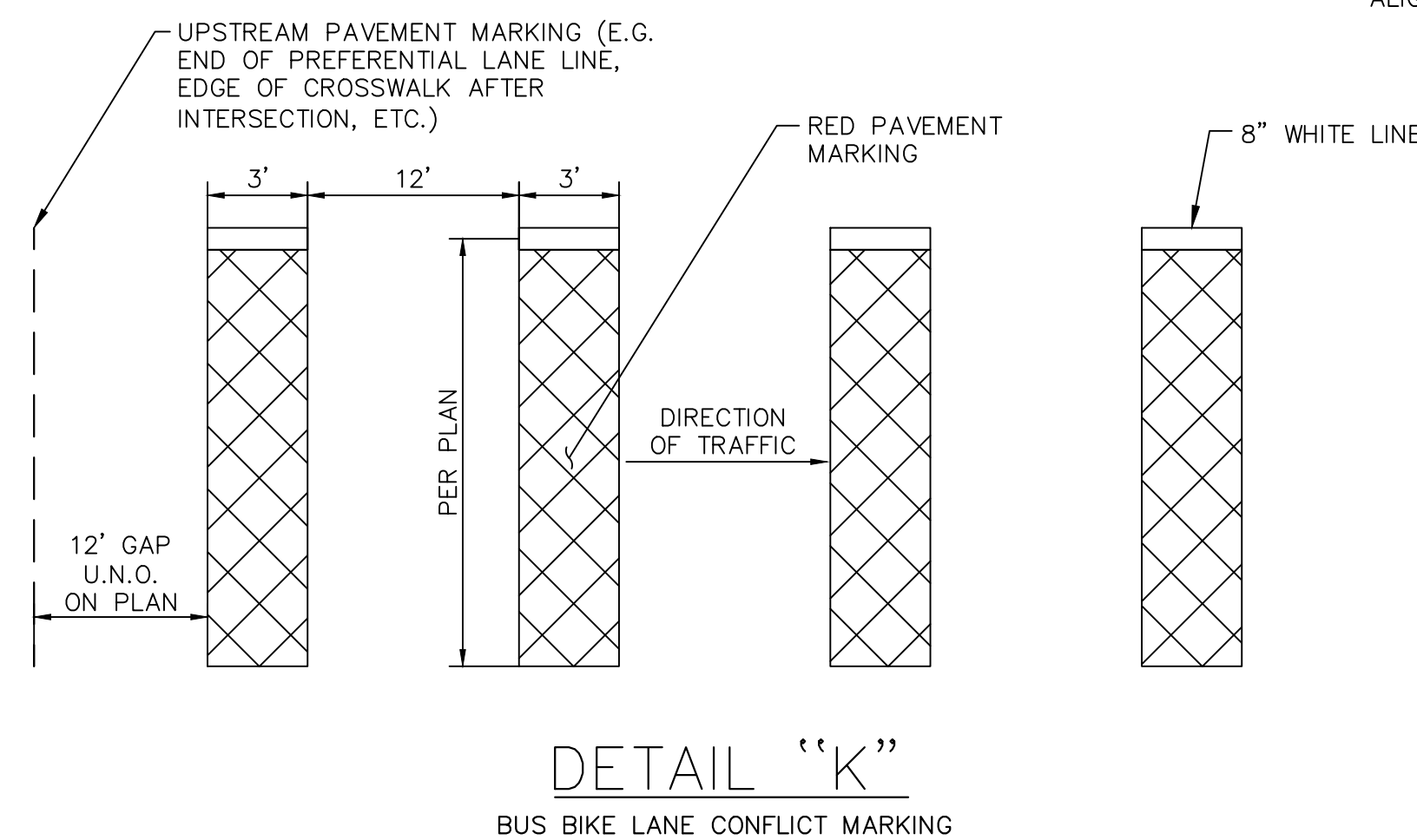
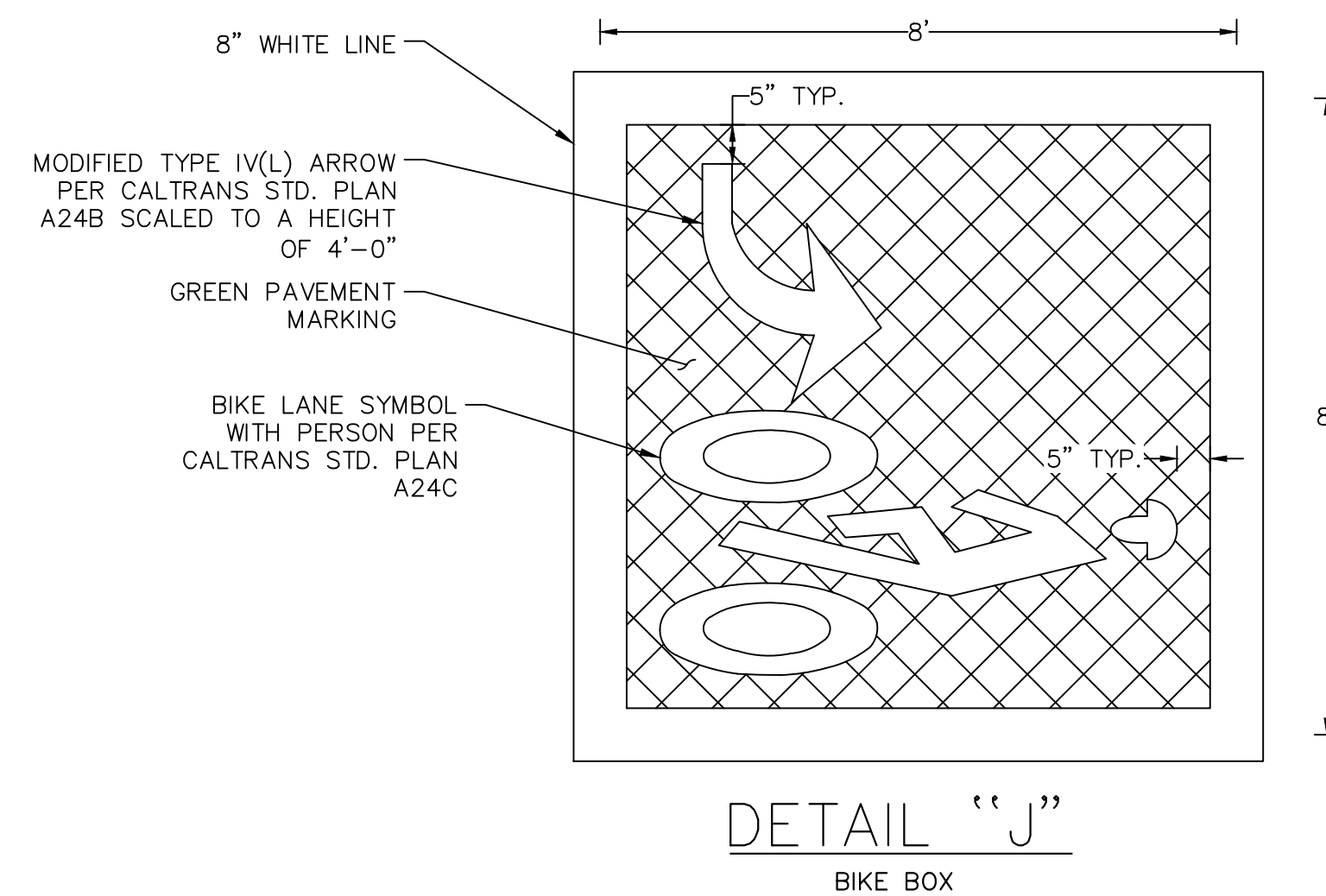
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3
OF
11
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PROJECT
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PLAN
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CITY OF CULVER CITY
PUBLIC WORKS DEPARTMENT
MOBILITY & TRAFFIC ENGINEERING DIVISION

MOVE CULVER CITY 2023
PHASE 2
DETAILS SHEET 1



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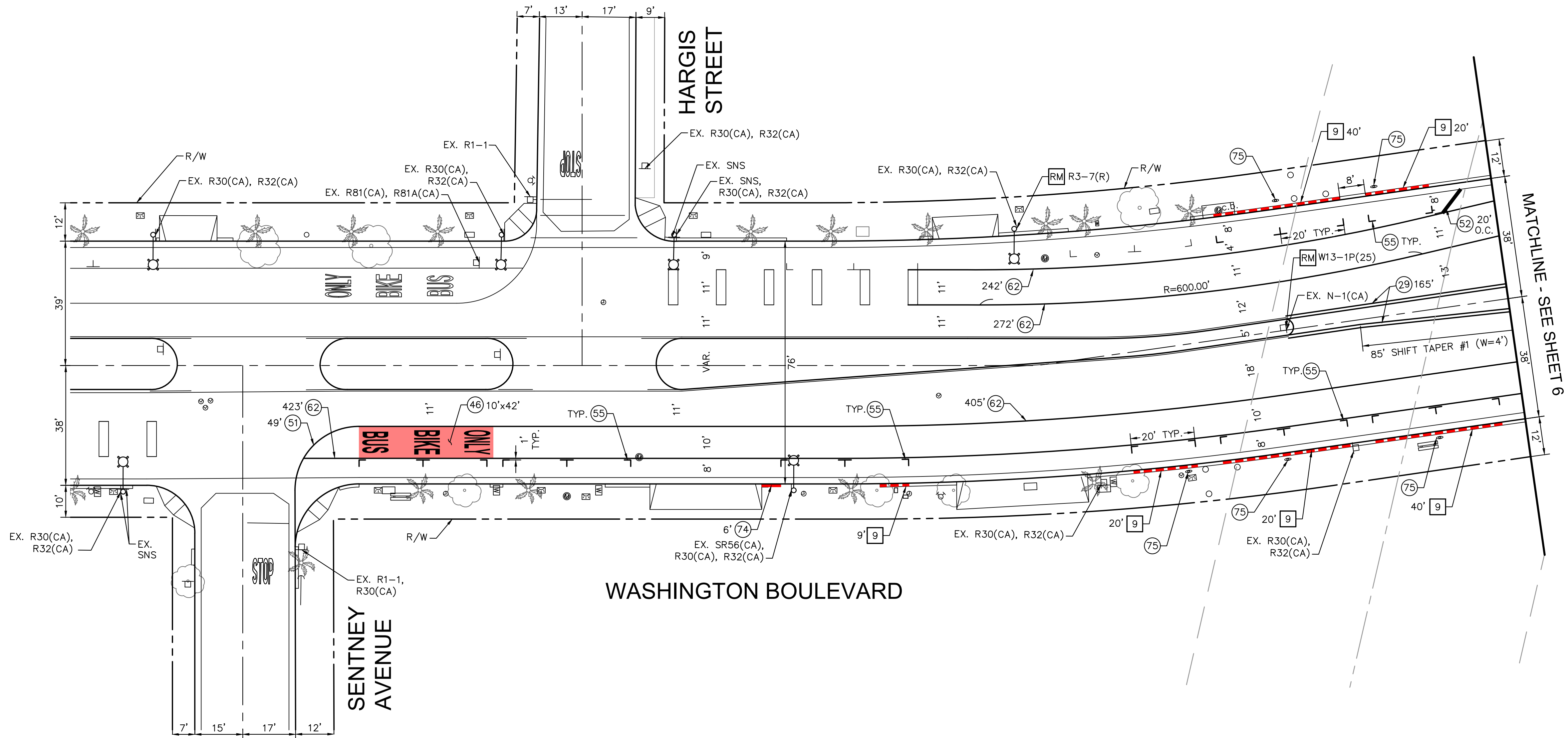
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DRAWN BY MJ,VC,RU	DATE 06/23/2025	APPROVED BY ANDREW MAXIMOUS, PE, TE MOBILITY & TRAFFIC ENGINEERING	DATE MANAGER			



LEGEND

- = RIGHT-OF-WAY
- = EXISTING STRIPING & MARKINGS TO REMAIN.
- = EXISTING STRIPING & MARKINGS TO BE REMOVED.
- = EXISTING PAVEMENT MARKING TO BE REMOVED.
- = PROPOSED STRIPING & MARKINGS.
- ⊥ = EXISTING SIGN AND POST.
- ⊥ = PROPOSED SIGN AND POST.
- ⊥ = PROPOSED ANGLED SIGN AND POST USING 30 DEGREE BRACKET.
- (S) = SIGNALIZED INTERSECTION.
- = EXISTING CURB MARKINGS TO REMAIN.
- = EXISTING CURB MARKINGS TO BE REMOVED.
- = PROPOSED CURB MARKINGS.
- = EXISTING DETECTION LOOP
- = EXISTING BICYCLE DETECTION LOOP
- 1/2" = DETAIL X | 1/2" = DETAIL Y PER CALTRANS STANDARD PLANS A20A-A20F
- = BEGINNING/END OF CURB RETURN

NOTES

- ALL EXISTING SIGNS TO REMAIN. PROTECT ALL EXISTING SIGNS IN PLACE.
- INSTALL PVMT. MARKINGS AS SHOWN.
- PAINT REMOVAL REQUIRED.

DEMOLITION NOTES:

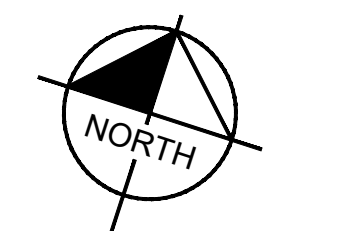
- 9 REMOVE EXISTING CURB MARKING BY WATER BLASTING OR CITY-APPROVED EQUAL.
- RM REMOVE AND DISCARD SIGN. REMOVE POST, IF NO SIGNS REMAINING.

CONSTRUCTION NOTES:

- 1 - (45) INSTALL STRIPING PER CORRESPONDING DETAIL 1-45 PER CALTRANS STANDARD PLAN A20A-A20F.
- (46) INSTALL RED-COLORED PAVEMENT MARKING PER DETAIL "L" ON SHEET 4.
- (51) INSTALL 8" WHITE LINE RIGHT TURN EDGELINE PER DETAIL "O" ON SHEET 4.
- (62) INSTALL 12" WHITE HATCHING AT 45 DEGREE ANGLE TO CURB FACE, SPACING PER PLAN.
- (55) INSTALL 4" WHITE, 1'-6" IN WIDTH AND 1'-6" IN LENGTH, PARALLEL PARKING STALL MARKING AS SHOWN ON PLAN.
- (62) INSTALL 8" WHITE LINE.
- (74) PAINT CURB RED. MEASURE LENGTH IN A STRAIGHT LINE.
- (75) FURNISH AND INSTALL METER POST. INSTALL CITY FURNISHED PARKING METER HEAD.

SIGN LEGEND

- N-1(CA)
- R1-1
- R3-7(R)
- R30(CA)
- R32(CA)
- R81(CA)
- R81B(CA)
- SR56(CA)
- W13-1P(25)



GRAPHIC SCALE IN FEET
0 10 20 40
1"=20' WHEN PLOTTED
FULL SIZE AT 24"x36"

CITY OF CULVER CITY
PUBLIC WORKS DEPARTMENT
MOBILITY & TRAFFIC ENGINEERING DIVISION

MOVE CULVER CITY 2023
PHASE 2
SIGNING AND STRIPING PLAN 1

NO	DESCRIPTION	BY	AP	DATE

PRELIMINARY
FOR REVIEW ONLY
NOT FOR
CONSTRUCTION
Kimley»Horn
Kimley-Horn and Associates, Inc.

PREPARED BY
Kimley»Horn
© 2025 KIMLEY-HORN AND ASSOCIATES, INC.
660 S. FIGUEROA STREET, SUITE 2050, LOS ANGELES, CA 90017
PHONE: 213-261-4040
WWW.KIMLEY-HORN.COM

DESIGNED BY TC,JM,MJ,VC	DATE 06/23/2025	CHECKED BY THOMAS CHECK, PE, TE SENIOR TRAFFIC ENGINEER	DATE
DRAWN BY MJ,VC,RU	DATE 06/23/2025	APPROVED BY ANDREW MAXIMOUS, PE, TE MOBILITY & TRAFFIC ENGINEERING MANAGER	DATE

SHEET 5 OF 11 SHEETS	PROJECT NUMBER PS017	PLAN NUMBER MTE1002
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Plotted By: Jacobson, Matthew Sheet Set: MOVE Culver City Layout: SS-2 Date: June 23, 2025 06:51:28pm Path: K:\LDT\PTO\JTS\099417014 - MOVE Culver City 2023\Design\CADD\PlanSheets\099417014-SS-PLAN-PHASE2-PROPOSED-01.dwg
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LEGEND

- = RIGHT-OF-WAY
— = EXISTING STRIPING & MARKINGS TO REMAIN.
- - - = EXISTING STRIPING & MARKINGS TO BE REMOVED.
[X] = EXISTING PAVEMENT MARKING TO BE REMOVED.
— = PROPOSED STRIPING & MARKINGS.
♣ = EXISTING SIGN AND POST.
♣ = PROPOSED SIGN AND POST.
♣ = PROPOSED ANGLED SIGN AND POST USING 30 DEGREE BRACKET.
S = SIGNALIZED INTERSECTION.
— = EXISTING CURB MARKINGS TO REMAIN.
— = EXISTING CURB MARKINGS TO BE REMOVED.
— = PROPOSED CURB MARKINGS.
○ = EXISTING DETECTION LOOP
○ = EXISTING BICYCLE DETECTION LOOP
X | Y = DETAIL X | DETAIL Y PER CALTRANS STANDARD PLANS A20A–A20F
/ = BEGINNING/END OF CURB RETURN

NOTES

- ALL EXISTING SIGNS TO REMAIN. PROTECT ALL EXISTING SIGNS IN PLACE.
- INSTALL PVMT. MARKINGS AS SHOWN.
- PAINT REMOVAL REQUIRED.

DEMOLITION NOTES:

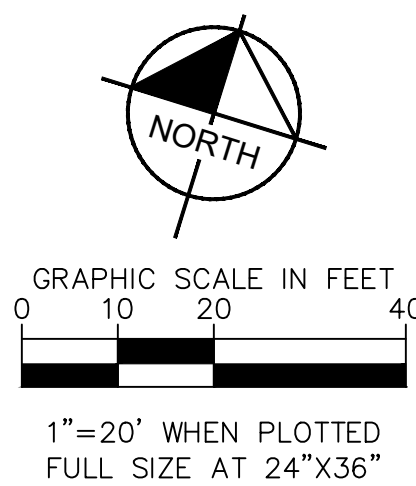
- 9 REMOVE EXISTING CURB MARKING BY WATER BLASTING OR CITY-APPROVED EQUAL.
RM REMOVE AND DISCARD SIGN. REMOVE POST, IF NO SIGNS REMAINING.

CONSTRUCTION NOTES:

- 1 - 45 INSTALL STRIPING PER CORRESPONDING DETAIL 1–45 PER CALTRANS STANDARD PLAN A20A–A20F.
46 INSTALL RED-COLORED PAVEMENT MARKING PER DETAIL "L" ON SHEET 4.
47 INSTALL 3' RED BUS LANE SKIP PER DETAIL "K" ON SHEET 4.
48 INSTALL 4" WHITE LANE LINE.
49 INSTALL MODIFIED PREFERENTIAL LANE LINE – 4" GAP PER DETAIL "G" ON SHEET 3.
51 INSTALL 8" WHITE LINE RIGHT TURN EDGELINE PER DETAIL "O" ON SHEET 4.
52 INSTALL 12" WHITE HATCHING AT 45 DEGREE ANGLE TO CURB FACE, SPACING PER PLAN.
55 INSTALL 4" WHITE, 1'–6" IN WIDTH AND 1'–6" IN LENGTH, PARALLEL PARKING STALL MARKING AS SHOWN ON PLAN.
61 INSTALL CURB EXTENSION PER DETAIL "N" ON SHEET 4.
62 INSTALL 8" WHITE LINE.
74 PAINT CURB RED. MEASURE LENGTH IN A STRAIGHT LINE.
75 FURNISH AND INSTALL METER POST. INSTALL CITY FURNISHED PARKING METER HEAD.
CW INSTALL 2' WIDE WHITE CONTINENTAL CROSSWALK PER DETAIL "M" ON SHEET 4. SIZE AND LOCATION PER PLAN.
IV INSTALL TYPE IV ARROW PER CALTRANS STANDARD PLAN A24A.
L INSTALL 12" WHITE LIMIT LINE. INSTALL 5' BACK (4.5' CLEAR) FROM CROSSWALK WHEN PRESENT. WHEN LIMIT LINE IS STAGGERED, 5' SETBACK SHALL BE MEASURED FROM CLOSEST EDGE OF CROSSWALK IN THAT LANE.

SIGN LEGEND

- RIGHT LANE MUST TURN RIGHT STOPPING ANY TIME STOPPING ANY TIME PARKING HOUR PARKING BUSES EXEMPT BUSES EXEMPT SIGN SINGLE LANE AHEAD
R3–7(R) R28(S)(CA)(DBL) R28(S)(CA)(R) R30(CA) R32(CA)



CITY OF CULVER CITY
PUBLIC WORKS DEPARTMENT
MOBILITY & TRAFFIC ENGINEERING DIVISION

MOVE CULVER CITY 2023
PHASE 2
SIGNING AND STRIPING PLAN 2

DESIGNED BY TC,JM,MJ,VC	DATE 06/23/2025	CHECKED BY THOMAS CHECK, PE, TE SENIOR TRAFFIC ENGINEER	DATE	SHEET 6
DRAWN BY M.J.VC,RU	DATE 06/23/2025	APPROVED BY ANDREW MAXIMOUS, PE, TE MOBILITY & TRAFFIC ENGINEERING MANAGER	DATE	OF 11 SHEETS
PROJECT NUMBER PS017	PLAN NUMBER MTE1002			

NO	DESCRIPTION	BY	AP	DATE

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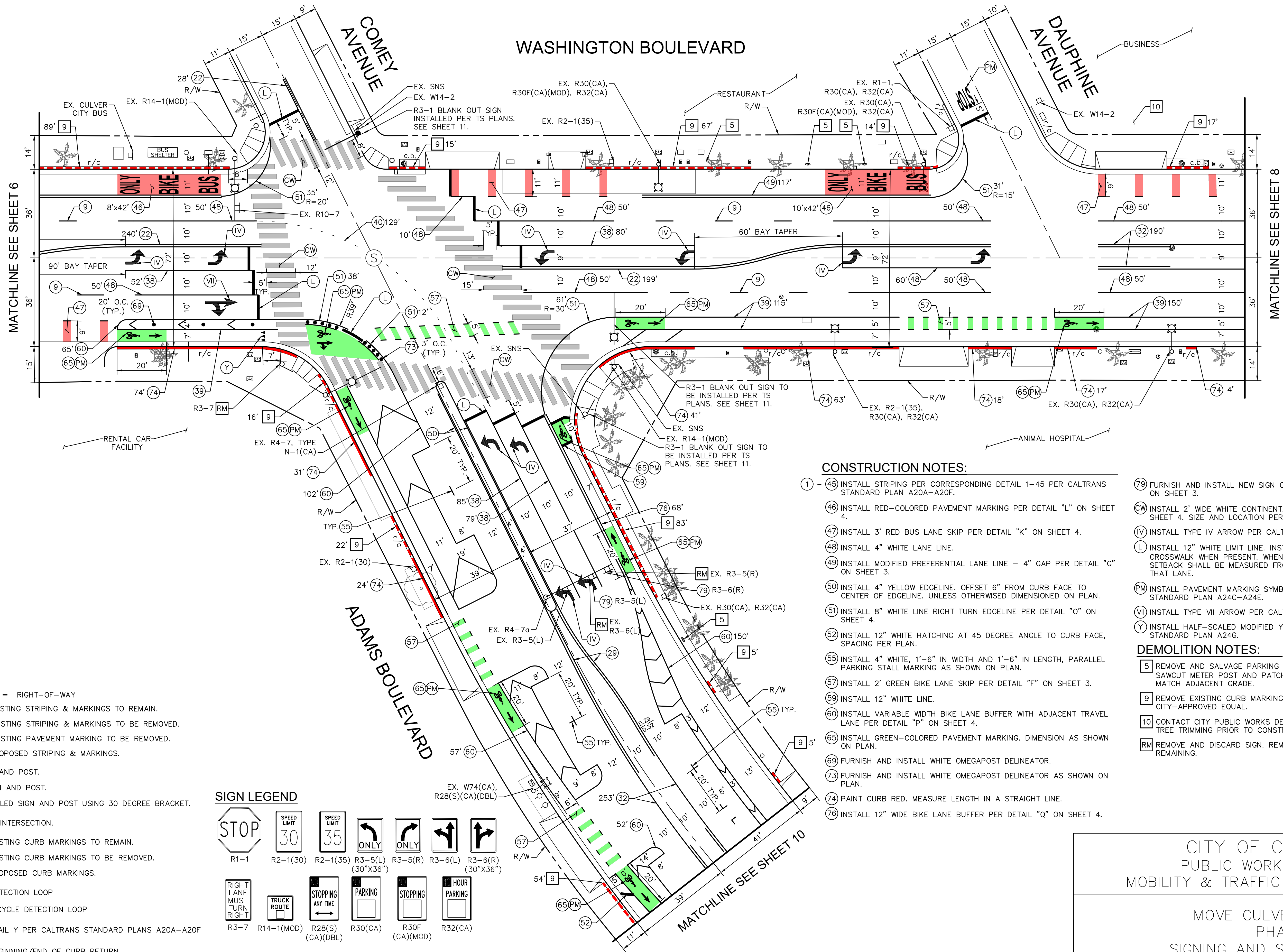
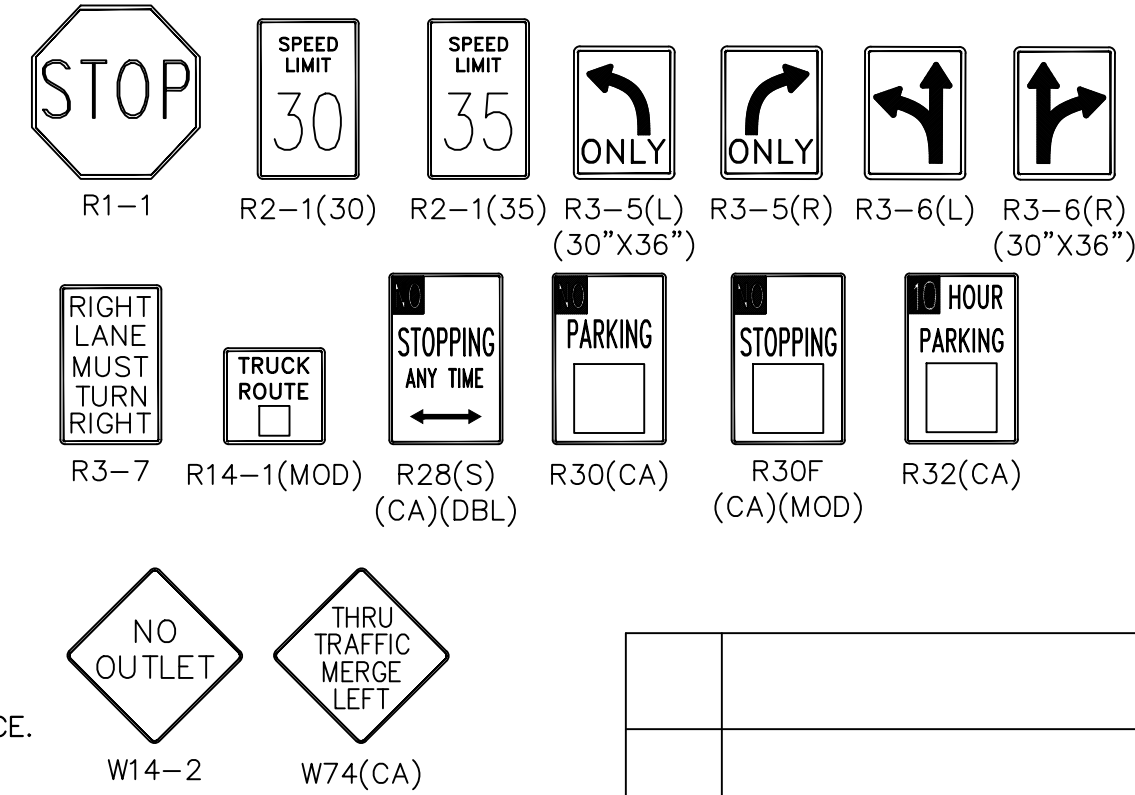
LEGEND

- = RIGHT-OF-WAY
— = EXISTING STRIPING & MARKINGS TO REMAIN.
--- = EXISTING STRIPING & MARKINGS TO BE REMOVED.
--- = EXISTING PAVEMENT MARKING TO BE REMOVED.
— = PROPOSED STRIPING & MARKINGS.
+ = EXISTING SIGN AND POST.
+ = PROPOSED SIGN AND POST.
+ = PROPOSED ANGLED SIGN AND POST USING 30 DEGREE BRACKET.
S = SIGNALIZED INTERSECTION.
— = EXISTING CURB MARKINGS TO REMAIN.
--- = EXISTING CURB MARKINGS TO BE REMOVED.
— = PROPOSED CURB MARKINGS.
○ = EXISTING DETECTION LOOP
○ = EXISTING BICYCLE DETECTION LOOP
X 1/10 DETAIL X 1/10 DETAIL Y PER CALTRANS STANDARD PLANS A20A-A20F
= BEGINNING/END OF CURB RETURN

NOTES

- ALL EXISTING SIGNS TO REMAIN. PROTECT ALL EXISTING SIGNS IN PLACE.
- INSTALL PVMT. MARKINGS AS SHOWN.
- PAINT REMOVAL REQUIRED.

SIGN LEGEND



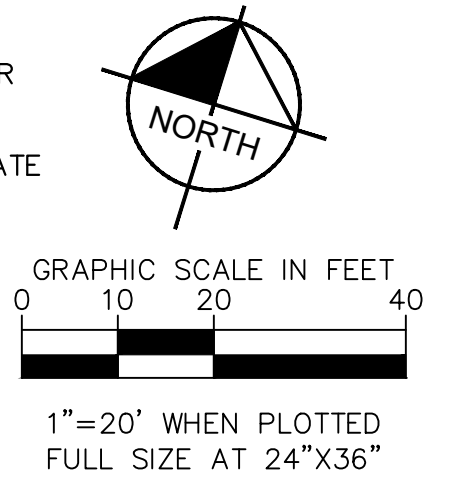
CONSTRUCTION NOTES:

- INSTALL STRIPING PER CORRESPONDING DETAIL 1-45 PER CALTRANS STANDARD PLAN A20A-A20F.
- INSTALL RED-COLORED PAVEMENT MARKING PER DETAIL "L" ON SHEET 4.
- INSTALL 3' RED BUS LANE SKIP PER DETAIL "K" ON SHEET 4.
- INSTALL 4" WHITE LANE LINE.
- INSTALL MODIFIED PREFERENTIAL LANE LINE - 4" GAP PER DETAIL "G" ON SHEET 3.
- INSTALL 4" YELLOW EDGE LINE. OFFSET 6" FROM CURB FACE TO CENTER OF EDGE LINE. UNLESS OTHERWISE DIMENSIONED ON PLAN.
- INSTALL 8" WHITE LINE RIGHT TURN EDGE LINE PER DETAIL "O" ON SHEET 4.
- INSTALL 12" WHITE HATCHING AT 45 DEGREE ANGLE TO CURB FACE, SPACING PER PLAN.
- INSTALL 4" WHITE, 1'-6" IN WIDTH AND 1'-6" IN LENGTH, PARALLEL PARKING STALL MARKING AS SHOWN ON PLAN.
- INSTALL 2' GREEN BIKE LANE SKIP PER DETAIL "F" ON SHEET 3.
- INSTALL 12" WHITE LINE.
- INSTALL VARIABLE WIDTH BIKE LANE BUFFER WITH ADJACENT TRAVEL LANE PER DETAIL "P" ON SHEET 4.
- INSTALL GREEN-COLORED PAVEMENT MARKING. DIMENSION AS SHOWN ON PLAN.
- FURNISH AND INSTALL WHITE OMEGAPOST DELINEATOR.
- FURNISH AND INSTALL WHITE OMEGAPOST DELINEATOR AS SHOWN ON PLAN.
- PAINT CURB RED. MEASURE LENGTH IN A STRAIGHT LINE.
- INSTALL 12" WIDE BIKE LANE BUFFER PER DETAIL "Q" ON SHEET 4.

- FURNISH AND INSTALL NEW SIGN ON EXISTING POST PER DETAIL "D" ON SHEET 3.
- INSTALL 2' WIDE WHITE CONTINENTAL CROSSWALK PER DETAIL "M" ON SHEET 4. SIZE AND LOCATION PER PLAN.
- INSTALL TYPE IV ARROW PER CALTRANS STANDARD PLAN A24A.
- INSTALL 12" WHITE LIMIT LINE. INSTALL 5' BACK (4.5' CLEAR) FROM CROSSWALK WHEN PRESENT. WHEN LIMIT LINE IS STAGGERED, 5' SETBACK SHALL BE MEASURED FROM CLOSEST EDGE OF CROSSWALK IN THAT LANE.
- INSTALL PAVEMENT MARKING SYMBOL/NUMERAL PER CALTRANS STANDARD PLAN A24C-A24E.
- INSTALL TYPE VII ARROW PER CALTRANS STANDARD PLAN A24A.
- INSTALL HALF-SCALED MODIFIED YIELD TRIANGLES PER CALTRANS STANDARD PLAN A24G.

DEMOLITION NOTES:

- REMOVE AND SALVAGE PARKING METER HEAD. CONTRACTOR TO SAWCUT METER POST AND PATCH FOR SMOOTH/LEVEL FINISH TO MATCH ADJACENT GRADE.
- REMOVE EXISTING CURB MARKING BY WATER BLASTING OR CITY-APPROVED EQUAL.
- CONTACT CITY PUBLIC WORKS DEPARTMENT TO COORDINATE TREE TRIMMING PRIOR TO CONSTRUCTION.
- REMOVE AND DISCARD SIGN. REMOVE POST, IF NO SIGNS REMAINING.



CITY OF CULVER CITY
PUBLIC WORKS DEPARTMENT
MOBILITY & TRAFFIC ENGINEERING DIVISION

MOVE CULVER CITY 2023
PHASE 2
SIGNING AND STRIPING PLAN 3

NO	DESCRIPTION	BY	AP	DATE

PRELIMINARY
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KimleyHorn
Kimley-Horn and Associates, Inc.

PREPARED BY

KimleyHorn
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660 S. FIGUEROA STREET, SUITE 2050, LOS ANGELES, CA 90017
PHONE: 213-261-4040
WWW.KIMLEY-HORN.COM

DESIGNED BY

DATE

TC, JM, MJ, VC

06/23/2025

DRAWN BY

DATE

MJ, VC, RU

06/23/2025

CHECKED BY

DATE

THOMAS CHECK, PE, TE

SENIOR TRAFFIC ENGINEER

APPROVED BY

DATE

ANDREW MAXIMOUS, PE, TE

MOBILITY & TRAFFIC ENGINEERING MANAGER

SHEET

7

OF

11

SHEETS

PROJECT

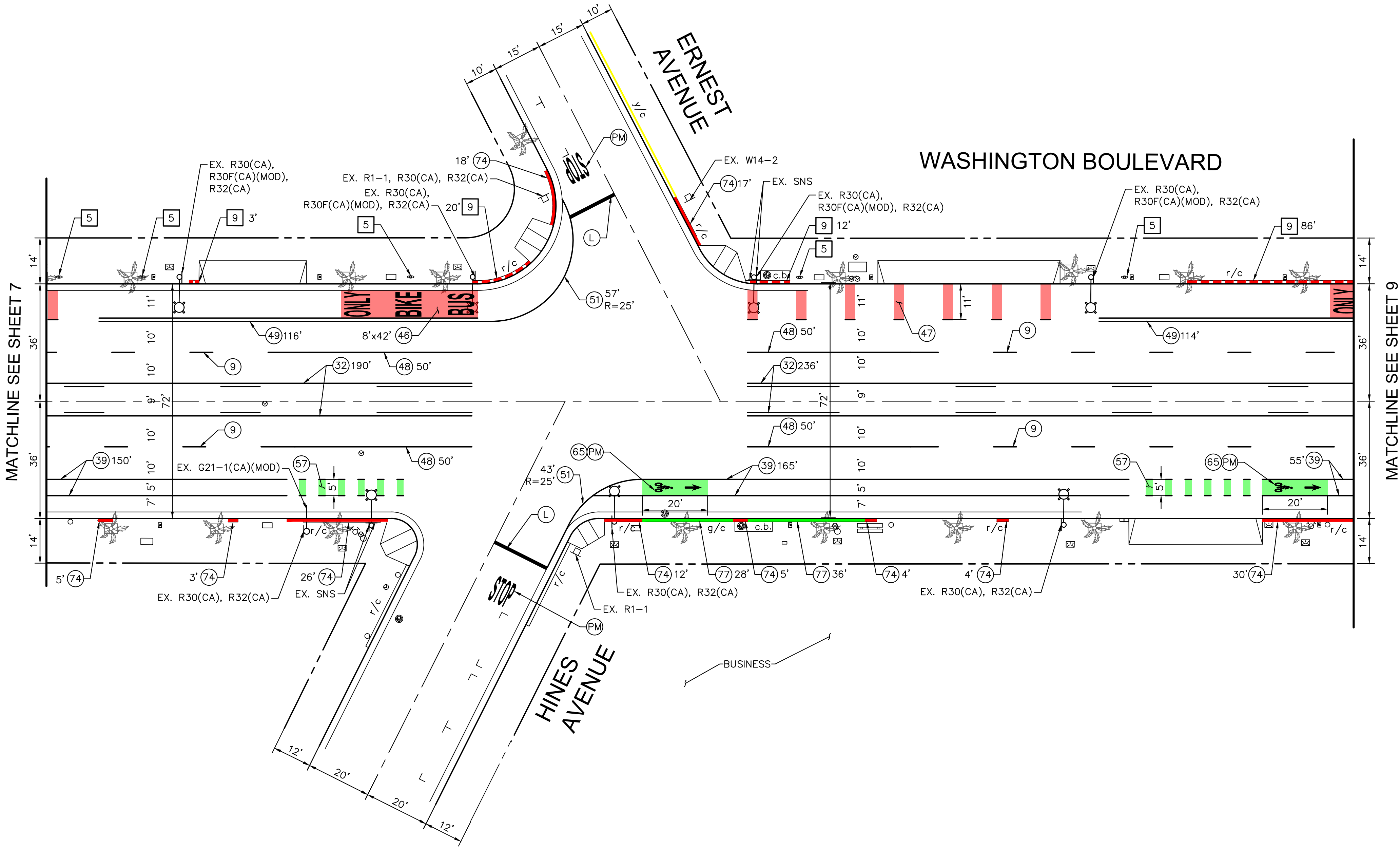
NUMBER

PS017

PLAN

NUMBER

MTE1002



LEGEND

- = RIGHT-OF-WAY
- = EXISTING STRIPING & MARKINGS TO REMAIN.
- = EXISTING STRIPING & MARKINGS TO BE REMOVED.
- = EXISTING PAVEMENT MARKING TO BE REMOVED.
- = PROPOSED STRIPING & MARKINGS.
- ⊥ = EXISTING SIGN AND POST.
- ⊥ = PROPOSED SIGN AND POST.
- ⊥ = PROPOSED ANGLED SIGN AND POST USING 30 DEGREE BRACKET.
- ⊙ = SIGNALIZED INTERSECTION.
- = EXISTING CURB MARKINGS TO REMAIN.
- = EXISTING CURB MARKINGS TO BE REMOVED.
- = PROPOSED CURB MARKINGS.
- ⊙ = EXISTING DETECTION LOOP
- ⊙ = EXISTING BICYCLE DETECTION LOOP
- | --- = DETAIL X | DETAIL Y PER CALTRANS STANDARD PLANS A20A-A20F
- = BEGINNING/END OF CURB RETURN

- NOTES
- ALL EXISTING SIGNS TO REMAIN. PROTECT ALL EXISTING SIGNS IN PLACE.
 - INSTALL PVMT. MARKINGS AS SHOWN.
 - PAINT REMOVAL REQUIRED.

DEMOLITION NOTES:

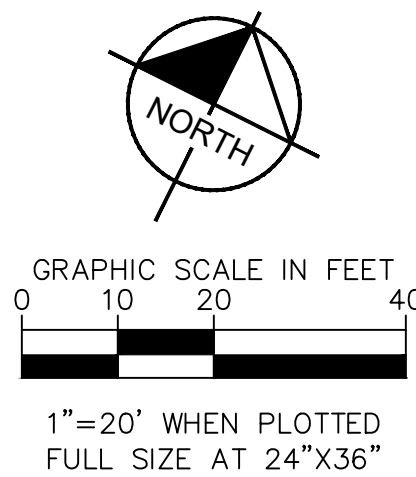
- 5 REMOVE AND SALVAGE PARKING METER HEAD. CONTRACTOR TO SAWCUT METER POST AND PATCH FOR SMOOTH/LEVEL FINISH TO MATCH ADJACENT GRADE.
- 9 REMOVE EXISTING CURB MARKING BY WATER BLASTING OR CITY-APPROVED EQUAL.

CONSTRUCTION NOTES:

- 1 - 45 INSTALL STRIPING PER CORRESPONDING DETAIL 1-45 PER CALTRANS STANDARD PLAN A20A-A20F.
- 46 INSTALL RED-COLORED PAVEMENT MARKING PER DETAIL "L" ON SHEET 4.
- 47 INSTALL 3' RED BUS LANE SKIP PER DETAIL "K" ON SHEET 4.
- 48 INSTALL 4" WHITE LANE LINE.
- 49 INSTALL MODIFIED PREFERENTIAL LANE LINE - 4" GAP PER DETAIL "G" ON SHEET 3.
- 51 INSTALL 8" WHITE LINE RIGHT TURN EDGELINE PER DETAIL "O" ON SHEET 4.
- 57 INSTALL 2' GREEN BIKE LANE SKIP PER DETAIL "F" ON SHEET 3.
- 65 INSTALL GREEN-COLORED PAVEMENT MARKING. DIMENSION AS SHOWN ON PLAN.
- 74 PAINT CURB RED. MEASURE LENGTH IN A STRAIGHT LINE.
- 77 PAINT CURB GREEN. MEASURE LENGTH IN A STRAIGHT LINE.
- L INSTALL 12" WHITE LIMIT LINE. INSTALL 5' BACK (4.5' CLEAR) FROM CROSSWALK WHEN PRESENT. WHEN LIMIT LINE IS STAGGERED, 5' SETBACK SHALL BE MEASURED FROM CLOSEST EDGE OF CROSSWALK IN THAT LANE.
- PM INSTALL PAVEMENT MARKING SYMBOL/NUMERAL PER CALTRANS STANDARD PLAN A24C-A24E.

SIGN LEGEND

- G21-1(CA)(MOD)
- R1-1
- R30(CA)
- R30F(CA)(MOD)
- R32(CA)
- W14-2



CITY OF CULVER CITY
PUBLIC WORKS DEPARTMENT
MOBILITY & TRAFFIC ENGINEERING DIVISION

MOVE CULVER CITY 2023
PHASE 2
SIGNING AND STRIPING PLAN 4

NO	DESCRIPTION	BY	AP	DATE

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CONSTRUCTION
Kimley»Horn
Kimley-Horn and Associates, Inc.

PREPARED BY
Kimley»Horn
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DESIGNED BY TC,JM,MJ,VC	DATE 06/23/2025	CHECKED BY THOMAS CHECK, PE, TE SENIOR TRAFFIC ENGINEER	DATE
DRAWN BY M.J.VC,RU	DATE 06/23/2025	APPROVED BY ANDREW MAXIMOUS, PE, TE MOBILITY & TRAFFIC ENGINEERING MANAGER	DATE

SHEET 8 OF 11 SHEETS	PROJECT NUMBER PS017	PLAN NUMBER MTE1002
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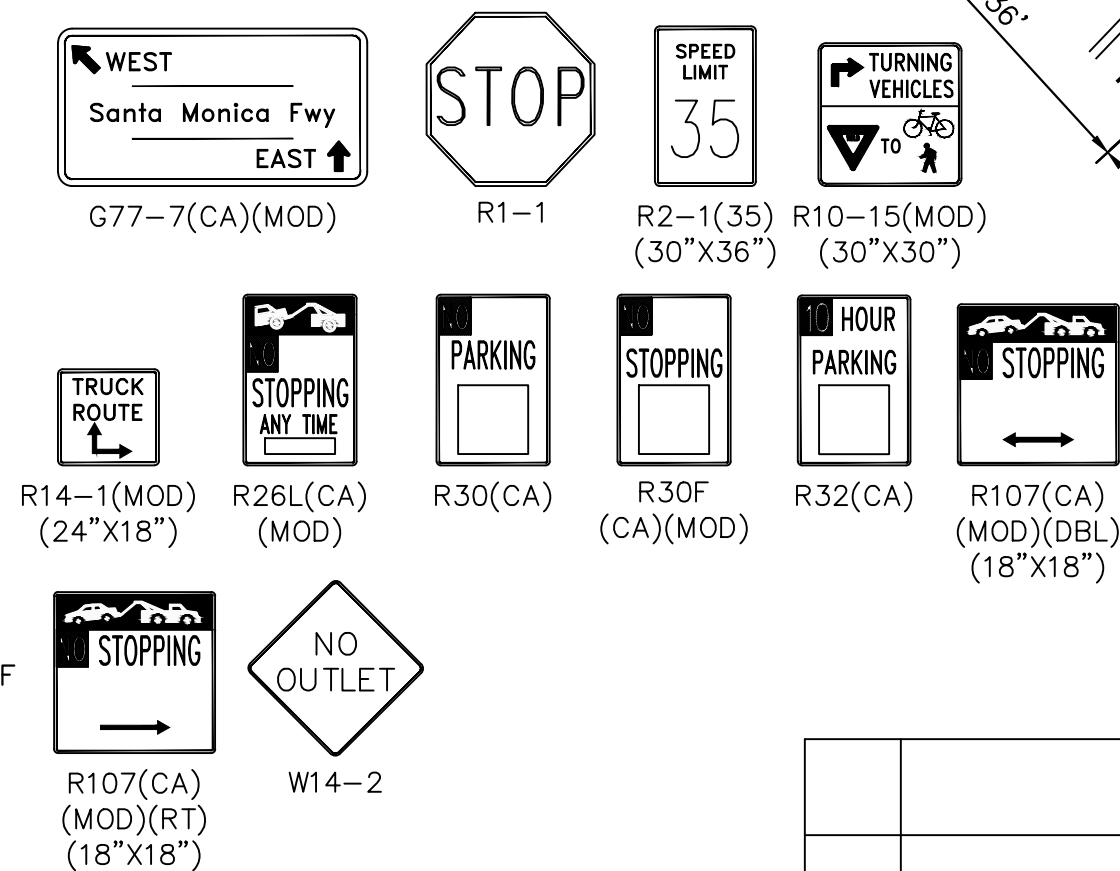
LEGEND

- RIGHT-OF-WAY
- EXISTING STRIPING & MARKINGS TO REMAIN.
- EXISTING STRIPING & MARKINGS TO BE REMOVED.
- EXISTING PAVEMENT MARKING TO BE REMOVED.
- PROPOSED STRIPING & MARKINGS.
- EXISTING SIGN AND POST.
- PROPOSED SIGN AND POST.
- PROPOSED ANGLED SIGN AND POST USING 30 DEGREE BRACKET.
- (S) = SIGNALIZED INTERSECTION.
- EXISTING CURB MARKINGS TO REMAIN.
- EXISTING CURB MARKINGS TO BE REMOVED.
- PROPOSED CURB MARKINGS.
- EXISTING DETECTION LOOP
- EXISTING BICYCLE DETECTION LOOP
- DETAIL X | DETAIL Y PER CALTRANS STANDARD PLANS A20A-A20F
- BEGINNING/END OF CURB RETURN

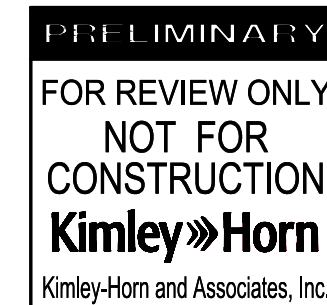
NOTES

- ALL EXISTING SIGNS TO REMAIN. PROTECT ALL EXISTING SIGNS IN PLACE.
- INSTALL P.V.M.T. MARKINGS AS SHOWN.
- PAINT REMOVAL REQUIRED.

SIGN LEGEND



NO	DESCRIPTION	BY	AP	DATE



PREPARED BY

Kimley»Horn

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DESIGNED BY

DATE

TC,JM,MJ,VC
06/23/2025

DRAWN BY

DATE

MJ,VC,RU
06/23/2025

CHECKED BY

THOMAS CHECK, PE, TE
SENIOR TRAFFIC ENGINEER

APPROVED BY

ANDREW MAXIMOUS, PE, TE
MOBILITY & TRAFFIC ENGINEERING MANAGER

SHEET

9

OF

11

SHEETS

PROJECT
NUMBER
PS017

PLAN
NUMBER
MTE1002

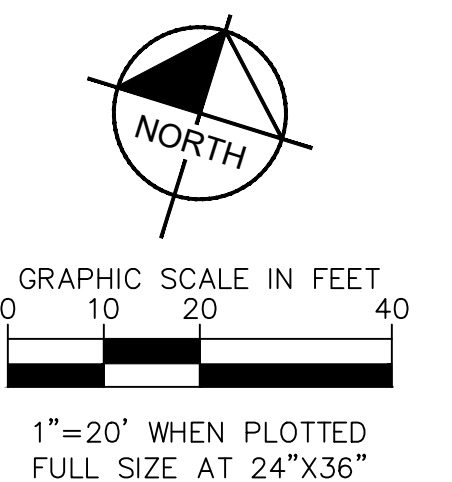
DEMOLITION NOTES:

- (9) REMOVE EXISTING CURB MARKING BY WATER BLASTING OR CITY-APPROVED EQUAL.
- (RM) REMOVE AND DISCARD SIGN. REMOVE POST, IF NO SIGNS REMAINING.

CONSTRUCTION NOTES:

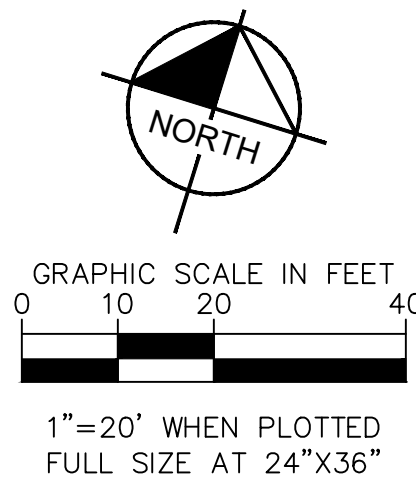
- (45) INSTALL STRIPING PER CORRESPONDING DETAIL 1-45 PER CALTRANS STANDARD PLAN A20A-A20F.
- (46) INSTALL RED-COLORED PAVEMENT MARKING PER DETAIL "L" ON SHEET 4.
- (48) INSTALL 4" WHITE LANE LINE.
- (51) INSTALL 8" WHITE LINE RIGHT TURN EDGELINE PER DETAIL "O" ON SHEET 4.
- (52) INSTALL 12" WHITE HATCHING AT 45 DEGREE ANGLE TO CURB FACE, SPACING PER PLAN.
- (57) INSTALL 2' GREEN BIKE LANE SKIP PER DETAIL "F" ON SHEET 3.
- (59) INSTALL 12" WHITE LINE.
- (62) INSTALL 8" WHITE LINE.
- (65) INSTALL GREEN-COLORED PAVEMENT MARKING. DIMENSION AS SHOWN ON PLAN.
- (68) INSTALL 8" WHITE CHEVRONS. LOCATION PER PLAN.
- (74) PAINT CURB RED. MEASURE LENGTH IN A STRAIGHT LINE.
- (78) FURNISH AND INSTALL NEW SIGN POST. RELOCATE EXISTING SIGN TO NEW POST.
- (79) FURNISH AND INSTALL NEW SIGN ON EXISTING POST PER DETAIL "D" ON SHEET 3.
- (80) FURNISH AND INSTALL NEW SIGN ON EXISTING TRAFFIC SIGNAL POLE PER DETAIL "E" ON SHEET 3.

- (CW) INSTALL 2' WIDE WHITE CONTINENTAL CROSSWALK PER DETAIL "M" ON SHEET 4. SIZE AND LOCATION PER PLAN.
- (IV) INSTALL TYPE IV ARROW PER CALTRANS STANDARD PLAN A24A.
- (L) INSTALL 12" WHITE LIMIT LINE. INSTALL 5' BACK (4.5' CLEAR) FROM CROSSWALK WHEN PRESENT. WHEN LIMIT LINE IS STAGGERED, 5' SETBACK SHALL BE MEASURED FROM CLOSEST EDGE OF CROSSWALK IN THAT LANE.
- (PM) INSTALL PAVEMENT MARKING SYMBOL/NUMERAL PER CALTRANS STANDARD PLAN A24C-A24E.
- (RL) RELOCATE EXISTING SIGN TO EXISTING POST.
- (VII) INSTALL TYPE VII ARROW PER CALTRANS STANDARD PLAN A24A.



CITY OF CULVER CITY
PUBLIC WORKS DEPARTMENT
MOBILITY & TRAFFIC ENGINEERING DIVISION

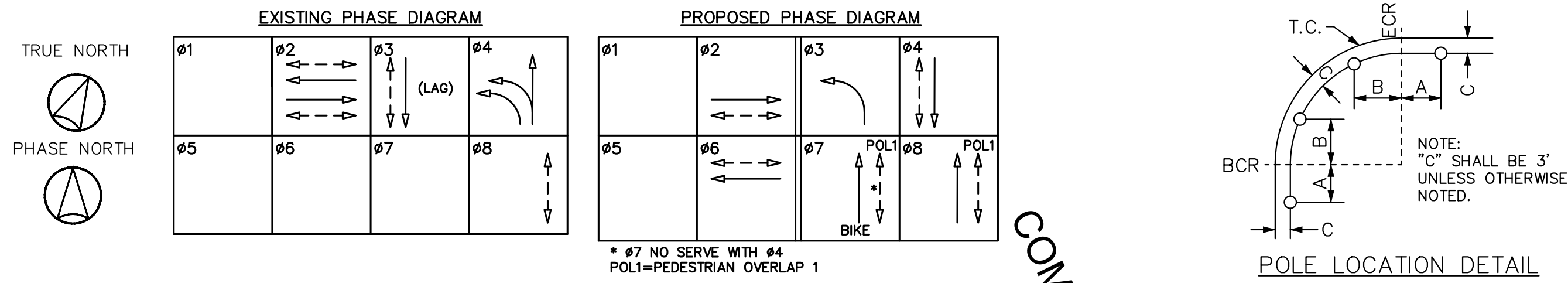
MOVE CULVER CITY 2023
PHASE 2
SIGNING AND STRIPING PLAN 5



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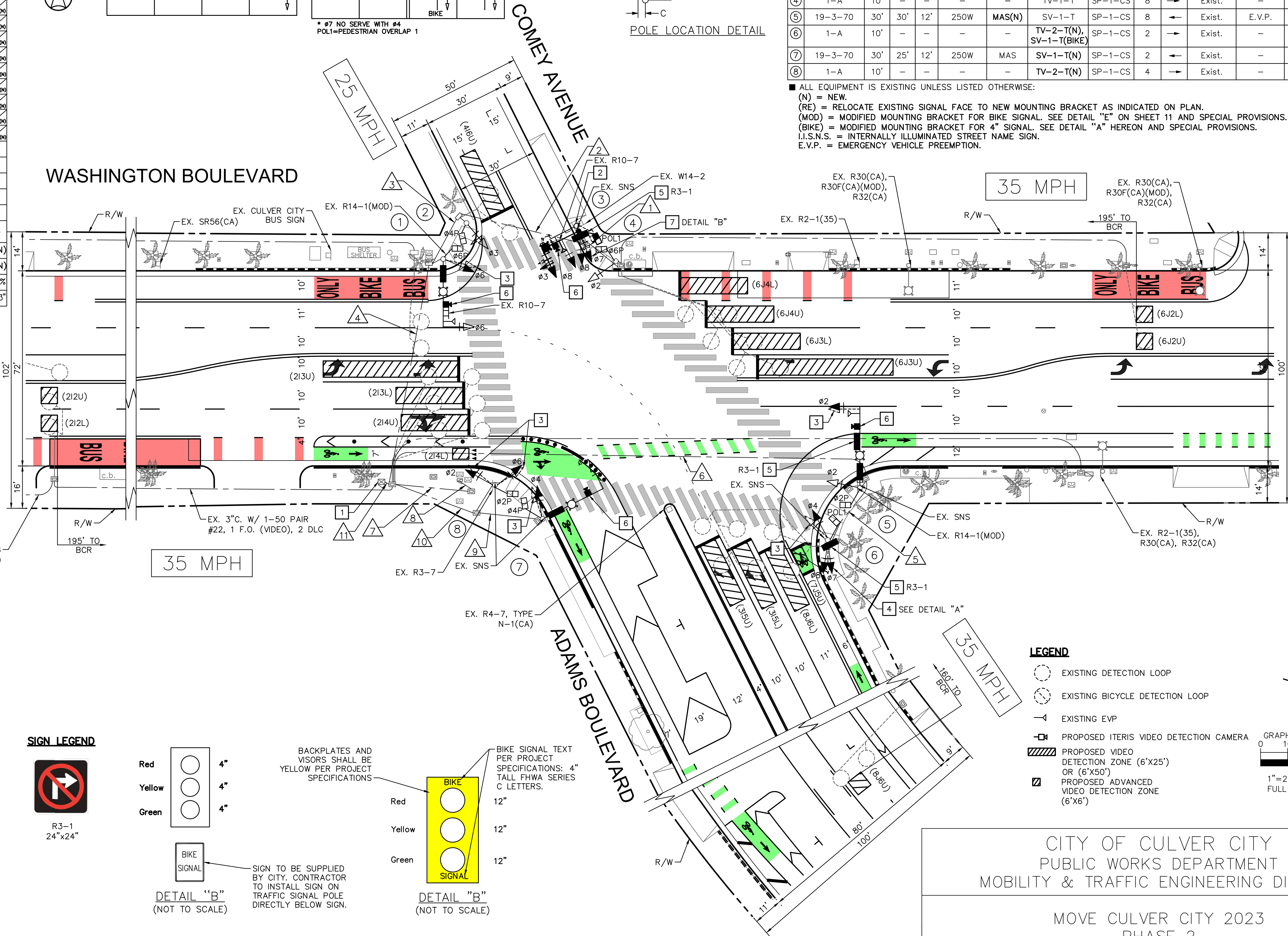
CONDUCTOR SCHEDULE													
AWG SIZE OR CABLE TYPE	POLE NO.	PHASES	CONDUIT										
			1	2	3	4	5	6	7	8	9	10	11
12 CSC	①	ø6,ø6P/ø4PPB	-	-	-	100	-	-	-	-	-	100	-
	②	ø3,ø4P/ø6PPB	-	-	-	100	-	-	-	-	-	100	-
	③	ø3,ø7,ø8,POL1/ø6PPB	-	-	100	100	100	-	-	-	-	100	-
	④	ø2,ø6P/ø8PPB	-	100	100	100	100	-	-	-	-	100	-
	⑤	ø2,ø2P/ø8PPB	100	100	100	100	-	-	-	-	-	100	-
	⑥	ø4,ø7,ø8,POL1/ø2PPB	-	-	-	-	100	100	-	-	-	100	-
	⑦	ø4,ø4P/ø2PPB	-	-	-	-	100	100	-	-	-	100	-
	⑧	ø2,ø6,ø2P/ø4PPB	-	-	-	-	-	-	-	100	100	100	-
	3 CSC	TOTAL	100	200	200	200	400	200	200	-	100	200	400
#10	SIGNAL COMMUNICATION	1	1	1	1	1	1	1	1	-	-	1	
#6	SIGNAL SERVICE	-	-	-	-	-	2	2	-	-	-	2	
#10	LUMINAIRE	-	2	2	2	-	2	2	2	2	-	2	
4#20	FIRE PRE-EMPT.	-	1	1	2	-	1	-	-	-	-	3	
6 PAIR #19 CABLE	INTERCONNECT	-	-	-	-	-	-	-	-	-	-	1	
#12	I.S.N.S.	-	1(N)	1(N)	1(N) ²	2	1(N) ³	1(N) ³	-	-	-	-	
VIDEO DETECTION CABLE (CATSE)		-	1(N)	1(N)	2(N)	-	1(N)	-	-	-	1(N)	4(N)	
BLANK-OUT CABLE (3 CSC)		-	1(N)	1(N)	1(N)	1(N)	2(N)	-	-	-	-	3(N)	
PERCENT FILL (%)		12%	32%	43%	26%	15%	36%	11%	11%	13%	11%	27%	
CONDUIT SIZE		2"	2"	2"	3"	2"	2"	2"	2"	2"	3"	2-3"	

▲ ALL CONDUCTORS/CABLES ARE EXISTING EXCEPT WHERE SHOWN.
(N)=NEW CABLES TO BE INSTALLED, EXISTING TO BE PROTECTED AND REUSED.
CONTRACTOR SHALL REMOVE ALL UNUSED CONDUCTORS, DLC'S, AND OTHER CABLES
FROM CONDUITS, PULLBOXES, AND SIGNAL POLES/MAST ARMS. ALL OTHER EXISTING
CONDUCTORS/CABLES SHALL BE PROTECTED AND MAINTAINED IN EXISTING CONDUIT.



POLE SCHEDULE																
No.	POLE DATA			SIGNAL M.A.	LUMINAIRE		SIGNAL MOUNTING			PED PUSH BUTN		POLE LOCATION			REMARK	I.I.S.N.S.
	TYPE	HEIGHT			M.A.	HPSV	M.A.	VEH.	PED.	PHASE	ARROW	A	B	C		
①	19-3-70	30'	25'	12'	250W	MAS	SV-1-T(N)	SP-1-CS	4	←	Exist.	E.V.P.	Cornex(N)			
②	1-A	10'	-	-	-	-	TV-1-T	SP-1-CS	6	→	Exist.	-	-			
③	17-3-100(N)	30'(N)	15'(N)	12'(N)	250W(N)	2-MAS(N)	SV-2-Tc(MOD)	SP-1-T(N)	6(N)	←	Exist.	E.V.P.(RE)	Washington(N)			
④	1-A	10'	-	-	-	-	TV-1-T	SP-1-CS	8	→	Exist.	-	-			
⑤	19-3-70	30'	30'	12'	250W	MAS(N)	SV-1-T	SP-1-CS	8	→	Exist.	E.V.P.	Adams(N)			
⑥	1-A	10'	-	-	-	-	TV-2-T(N), SV-1-T(BIKE)	SP-1-CS	2	→	Exist.	-	-			
⑦	19-3-70	30'	25'	12'	250W	MAS	SV-1-T(N)	SP-1-CS	2	←	Exist.	-	Washington(N)			
⑧	1-A	10'	-	-	-	-	TV-2-T(N)	SP-1-CS	4	→	Exist.	-	-			

■ ALL EQUIPMENT IS EXISTING UNLESS LISTED OTHERWISE:
(N) = NEW.
(RE) = RELOCATE EXISTING SIGNAL FACE TO NEW MOUNTING BRACKET AS INDICATED ON PLAN.
(MOD) = MODIFIED MOUNTING BRACKET FOR BIKE SIGNAL. SEE DETAIL "E" ON SHEET 11 AND SPECIAL PROVISIONS.
(BIKE) = MODIFIED MOUNTING BRACKET FOR 4" SIGNAL. SEE DETAIL "A" HEREON AND SPECIAL PROVISIONS.
I.I.S.N.S. = INTERNALLY ILLUMINATED STREET NAME SIGN.
E.V.P. = EMERGENCY VEHICLE PREEMPTION.



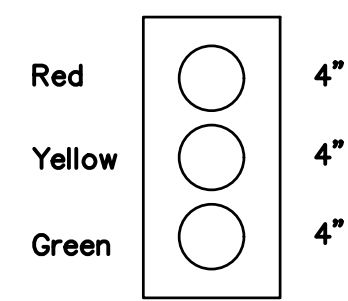
CONSTRUCTION NOTES:

- 1 CONTRACTOR SHALL ENSURE FIELD TERMINATIONS AND CABINET WIRING SUPPORT INTENDED FOR THE PHASING AND OPERATION AS SHOWN ON THIS PLAN.
- 2 REMOVE AND SALVAGE EXISTING TRAFFIC SIGNAL POLE AND FOUNDATION COMPLETE.
- 3 REMOVE AND SALVAGE EXISTING VEHICULAR SIGNAL HEAD AND MOUNTING BRACKET.
- 4 FURNISH AND INSTALL 4" SIGNAL FACE 4.5' ABOVE SIDEWALK. SIGNAL INDICATIONS SHALL BE AIMED 90 DEGREES AWAY FROM CURB FACE AS SHOWN ON PLAN.
- 5 INSTALL CITY-FURNISHED BLANK-OUT SIGN. EACH BLANK OUT SIGN SHALL TERMINATE TO A SEPARATE CHANNEL WITHIN THE CABINET TO ENABLE SIGNS TO BE OPERATED INDEPENDENTLY OF ONE ANOTHER.
- 6 FURNISH AND INSTALL ITERIS VIDEO DETECTION CAMERA PER MANUFACTURER SPECIFICATIONS AND AS DIRECTED BY ENGINEER. CAMERA SYSTEM SHALL INCLUDE CAMERA, MOUNTING HARDWARE (CAMERA MOUNTING BRACKET OR POLE MOUNT EXTENSION BRACKET), VIDEO DETECTION CABLE AND ANY ASSOCIATED EQUIPMENT FOR THE INTENDED OPERATION.
- 7 FURNISH AND INSTALL 12" LOUVERED SIGNAL FACE. AIM SIGNAL FACE AS SHOWN ON PLAN.

SIGN LEGEND



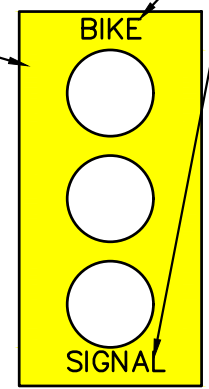
R3-1
24"x24"



BIKE

DETAIL "B"
(NOT TO SCALE)

BACKPLATES AND VISORS SHALL BE YELLOW PER PROJECT SPECIFICATIONS-



DETAIL "B"
(NOT TO SCALE)

UTILITY NOTICE

THE EXISTENCE OR LOCATION OF ANY UNDERGROUND UTILITIES, PIPES, STRUCTURES, ETC. WHICH WERE NOT VISIBLE FOR FIELD VERIFICATION AND ARE SHOWN ON THIS PLAN SET, WERE SHOWN FROM INFORMATION PROVIDED BY OTHERS NOT CONNECTED WITH KIMLEY-HORN AND ASSOCIATES. THE UNDERGROUND UTILITIES ARE SHOWN FOR INFORMATION PURPOSES ONLY AND DO NOT REPRESENT PRECISE LOCATIONS OR DEPTHS. ANY CONSTRUCTION RELATIVE TO UNDERGROUND UTILITIES MUST REQUIRE THE CONTRACTOR OR OTHERS TO ASCERTAIN THE EXISTENCE AND TRUE LOCATION AND DEPTH OF ANY UNDERGROUND UTILITIES. VERIFICATION OF LOCATION AND DEPTH OF UNDERGROUND UTILITIES PRIOR TO CONSTRUCTION WILL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. NONCOMPLIANCE OF VERIFICATION HOLDS KIMLEY-HORN AND ASSOCIATES HARMLESS FROM ANY DAMAGES OR LOSSES INCURRED.

NO	DESCRIPTION	BY	AP	DATE

PRELIMINARY
FOR REVIEW ONLY
NOT FOR
CONSTRUCTION
Kimley»Horn
Kimley-Horn and Associates, Inc.

PREPARED BY	
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Kimley»Horn

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660 S. FIGUEROA STREET, SUITE 2050, LOS ANGELES, CA 90017
PHONE: 213-261-4040
WWW.KIMLEY-HORN.COM

DESIGNED BY TC,JM,MJ,VC	DATE 06/23/2025	CHECKED BY THOMAS CHECK, PE, TE SENIOR TRAFFIC ENGINEER	SHEET 11	PROJECT NUMBER PS017	PLAN NUMBER MTE1002
DRAWN BY MJ,VC,RU	DATE 06/23/2025	APPROVED BY ANDREW MAXIMOUS, PE, TE MOBILITY & TRAFFIC ENGINEERING MANAGER	OF 11 SHEETS		